

Appendix C.5

Consultation Record

APPENDIX A: NOTICES

Notice of Online Open House #4

Individual Environmental Assessment Study

Teston Road Area Improvements

The Regional Municipality of York is conducting an Individual Environmental Assessment (IEA) Study in the City of Vaughan to examine transportation improvements in the Teston Road area between Highway 400 and Bathurst Street and Major Mackenzie Drive and Kirby Road.

WE WANT TO HEAR FROM YOU

You are invited to take part in the fourth online open house to learn about the study process, design considerations, preliminary design, potential impacts and mitigation measures for the following sections of the project:

- Teston Road from Keele Street to Rodinea Road
- East of Rodinea Road to the proposed valley crossing of the East Don River tributary
- Valley crossing of the East Don River tributary to Dufferin Street
- Dufferin Street to Bathurst Street

You can provide comments through an online survey at york.ca/TestonRoad. The survey is available between **Friday December 1, 2023 to Friday, December 22, 2023**.

To submit a question, comment or request to be added to the project mailing list, please contact:

YORK REGION

Public Works

17250 Yonge Street, Newmarket, Ontario, L3Y 6Z1

Phone: 1-877-464-9675 ext. 75000

TTY: 1-866-512-6228

Email: transportation@york.ca

To better assist you, please quote **Teston Road IEA** in your inquiry.

This study will be carried out according to the approved Terms of Reference and the requirements of the *Ontario Environmental Assessment Act*. Personal information submitted (e.g. name, address and phone number) is collected, maintained and disclosed under the authority of the *Ontario Environmental Assessment Act* and the *Municipal Freedom of Information and Protection of Privacy Act* for transparency and consultation purposes. Personal information you submit will become part of the public record that is available to the general public, unless you request that your personal information remain confidential.

Wayne Emmerson

York Region Chairman and CEO



APPENDIX B: PRESENTATION MATERIALS

TESTON ROAD AREA TRANSPORTATION IMPROVEMENTS

Individual Environmental Assessment – Online Open House #4

Recap of Open House #3

December 2023

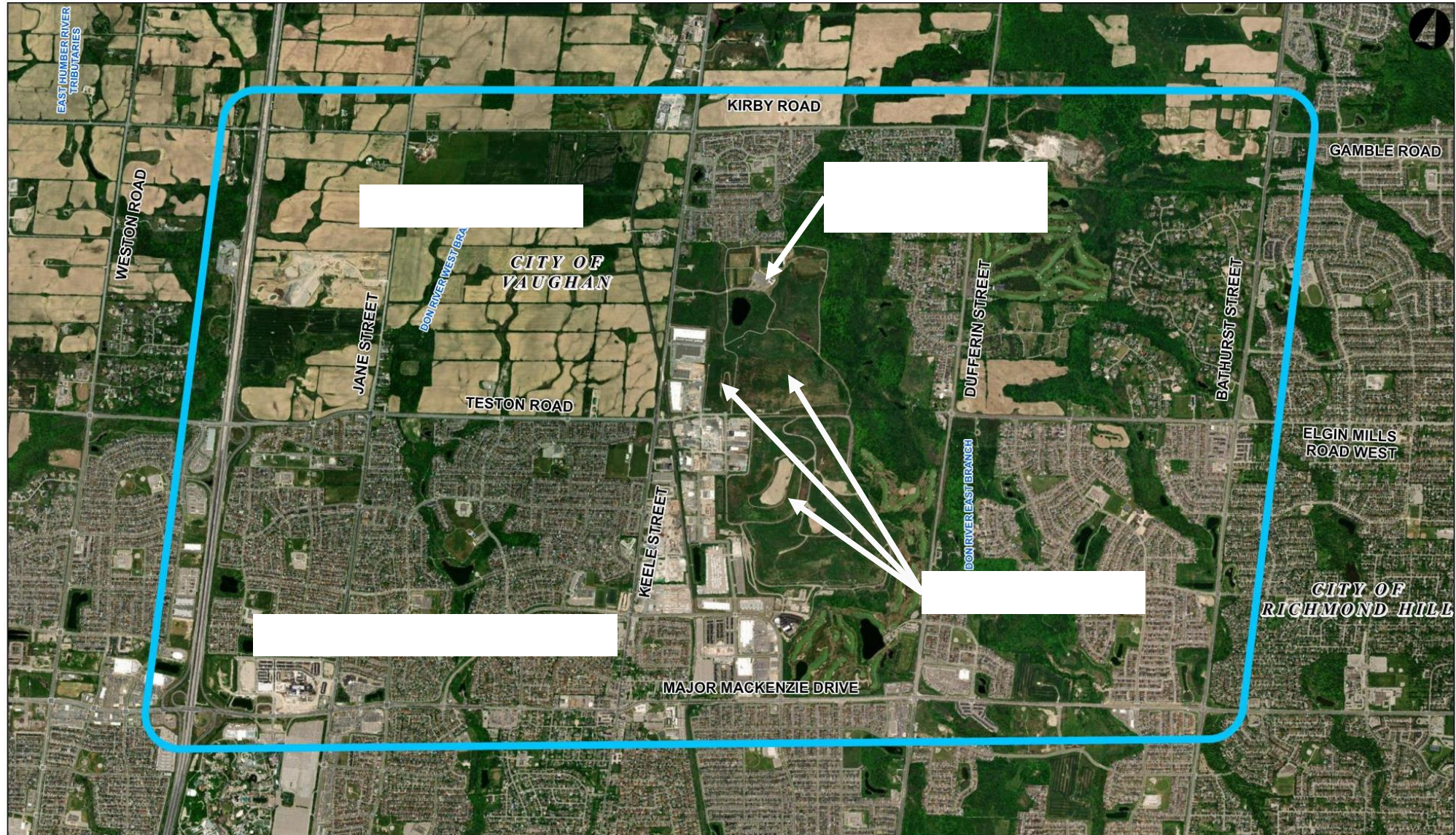
Study Background

- The “missing link” area of Teston Road between Keele Street and Dufferin Street will be studied as an Individual Environmental Assessment (IEA)
- IEAs are the highest level of EA in Ontario and are reserved for complex projects with the potential for significant environmental effects
- The first stage of an IEA is to complete a Terms of Reference (ToR) which establishes the planning and decision-making process for the subsequent IEA study
- Ministry of the Environment, Conservation and Parks (MECP) approved the Teston Road Area ToR in 2018
- York Region is proceeding with the IEA in accordance with the ToR which can be viewed at www.York.ca/TestonRoad

Problem and Opportunity Statement

To improve the efficiency, safety and continuity of the transportation network within the Teston Road area.

Study Area — Existing Conditions

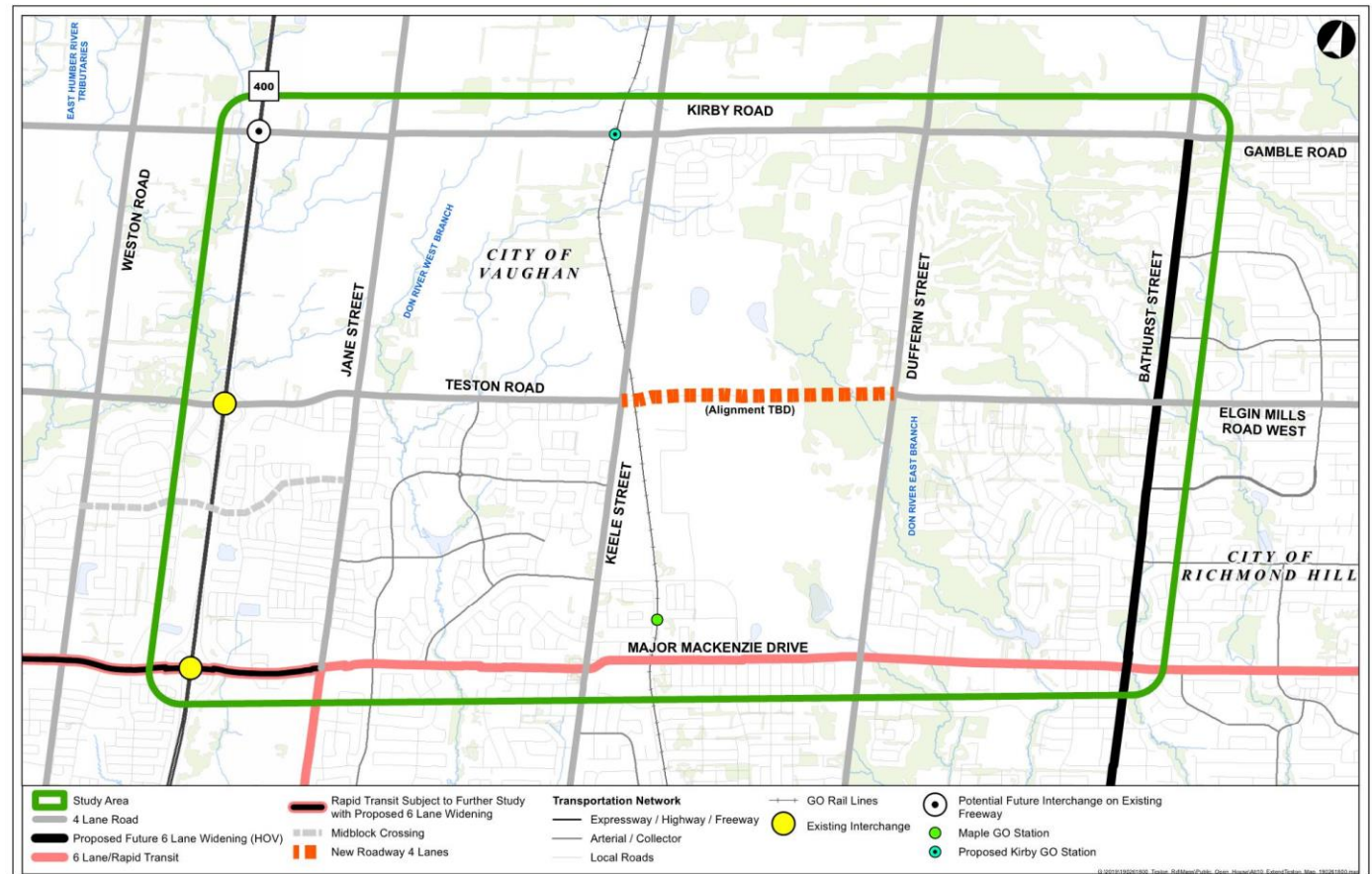


Generation and Evaluation of Alternative Corridors

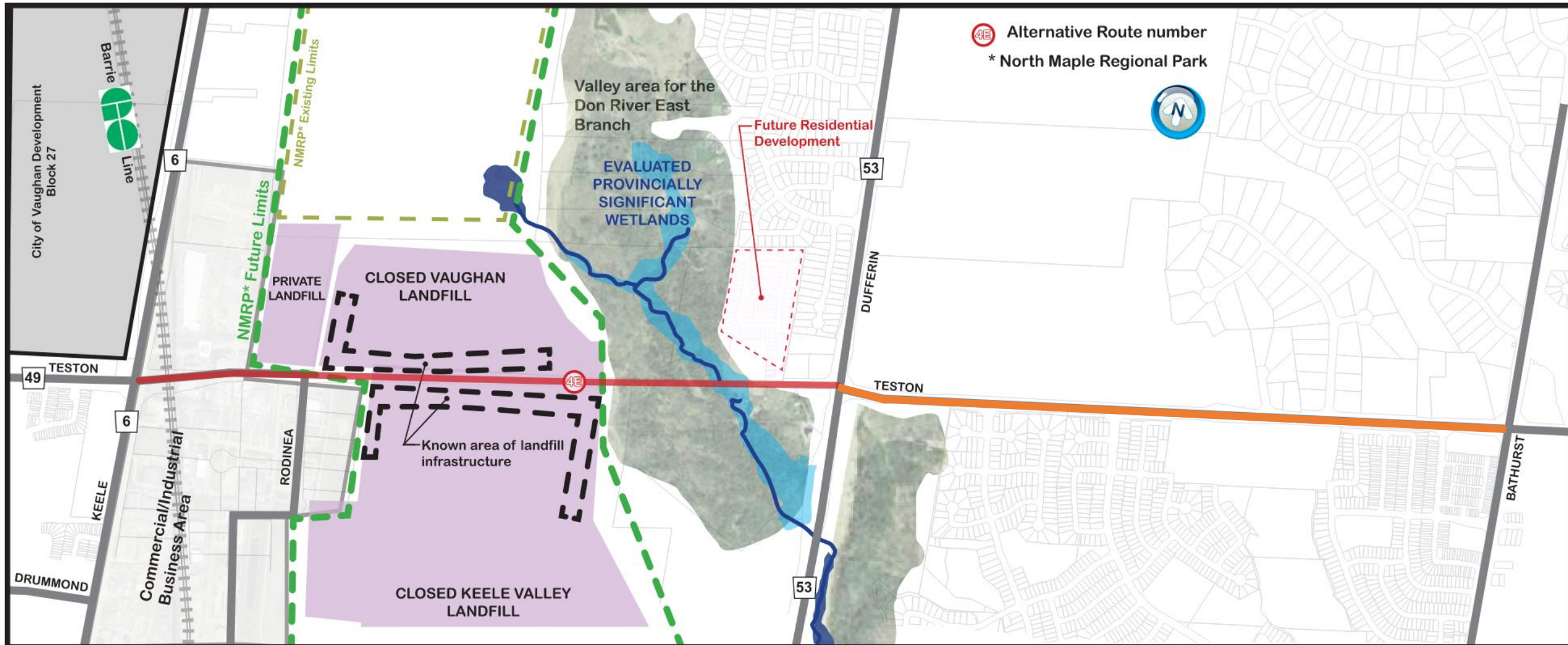
- Open house #1 presented alternatives to the undertaking
- Open house #2 presented alternative methods (alignment alternatives)
 - Alternative corridors: Different methods of achieving the recommended alternative to the undertaking
 - Alternative alignments: Different methods of achieving the recommended alternative corridor
- Open house #3 presented design alternatives

Preferred Alternative to the Undertaking

During open house #1, a new four-lane Teston Road connection between Keele Street and Dufferin Street with active transportation and potential for transit service (alternative 4) was confirmed as the preferred alternative to the undertaking.

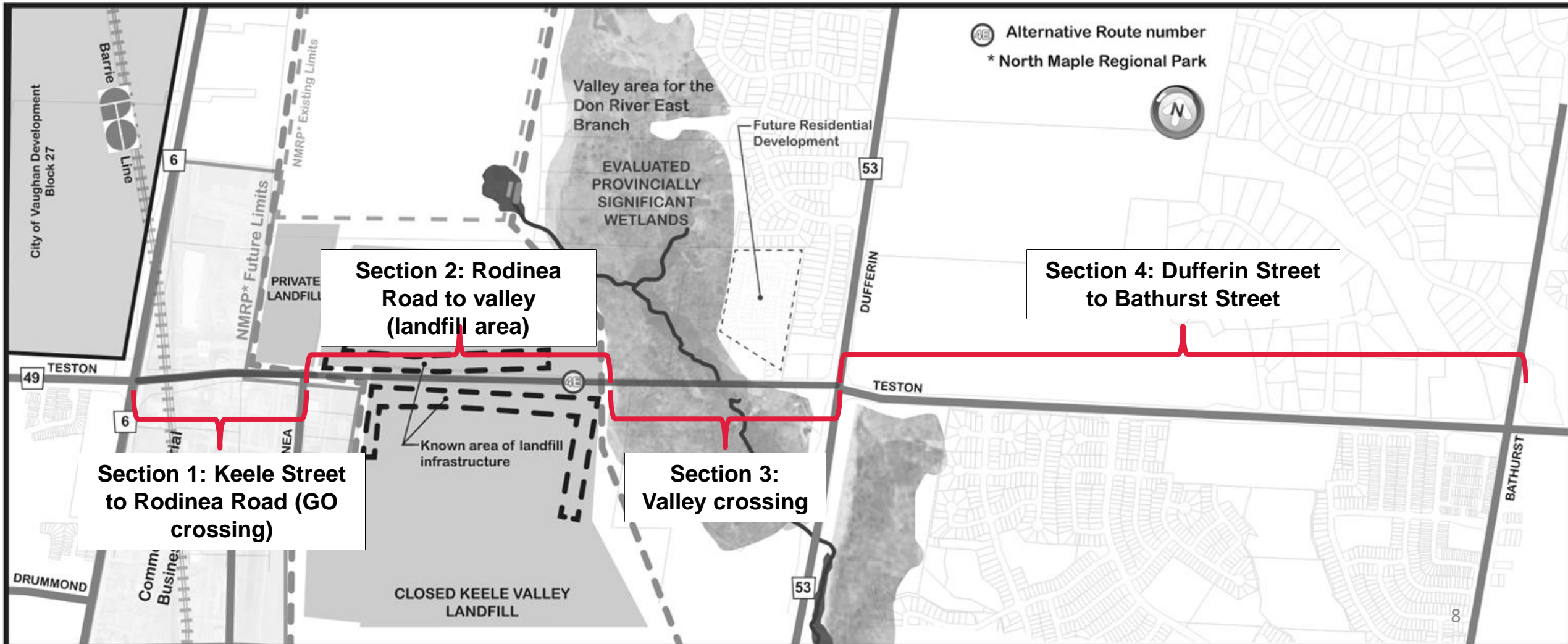


Preferred Alternative Alignment

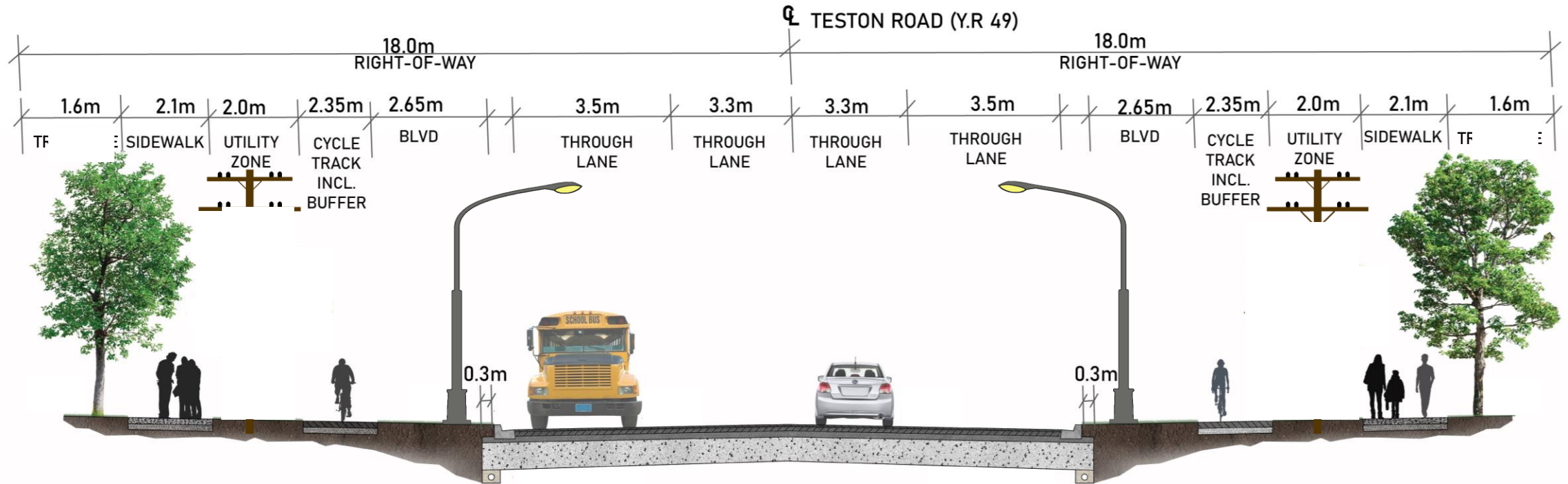


Generation and Evaluation of Design Alternatives

- Four Sections of the project with unique design challenges requiring solutions



Future Full Width Cross Section (For All Sections)



Section 1: Keele Street To Rodinea Road (GO Rail Crossing)

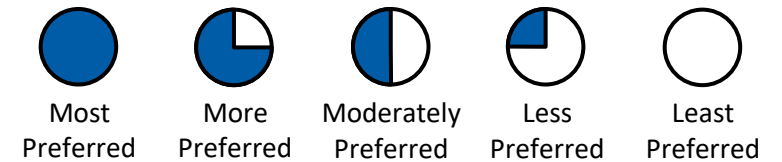
- **Grade-separated GO rail crossing options:**
 - Road-under-rail options were screened out as they would be more costly, more difficult to construct and maintain, and more disruptive to rail service during construction



- **Five alternatives were carried forward for a grade-separated GO rail crossing:**
 1. Existing Teston Road, existing Keele Street, overpass
 2. Shift Teston Road north, existing Keele Street, overpass
 3. Existing Teston Road, shift Keele Street west, overpass
 4. Shift Teston Road north, shift Keele Street West, overpass
 5. Future do-nothing

Section 1 Evaluation

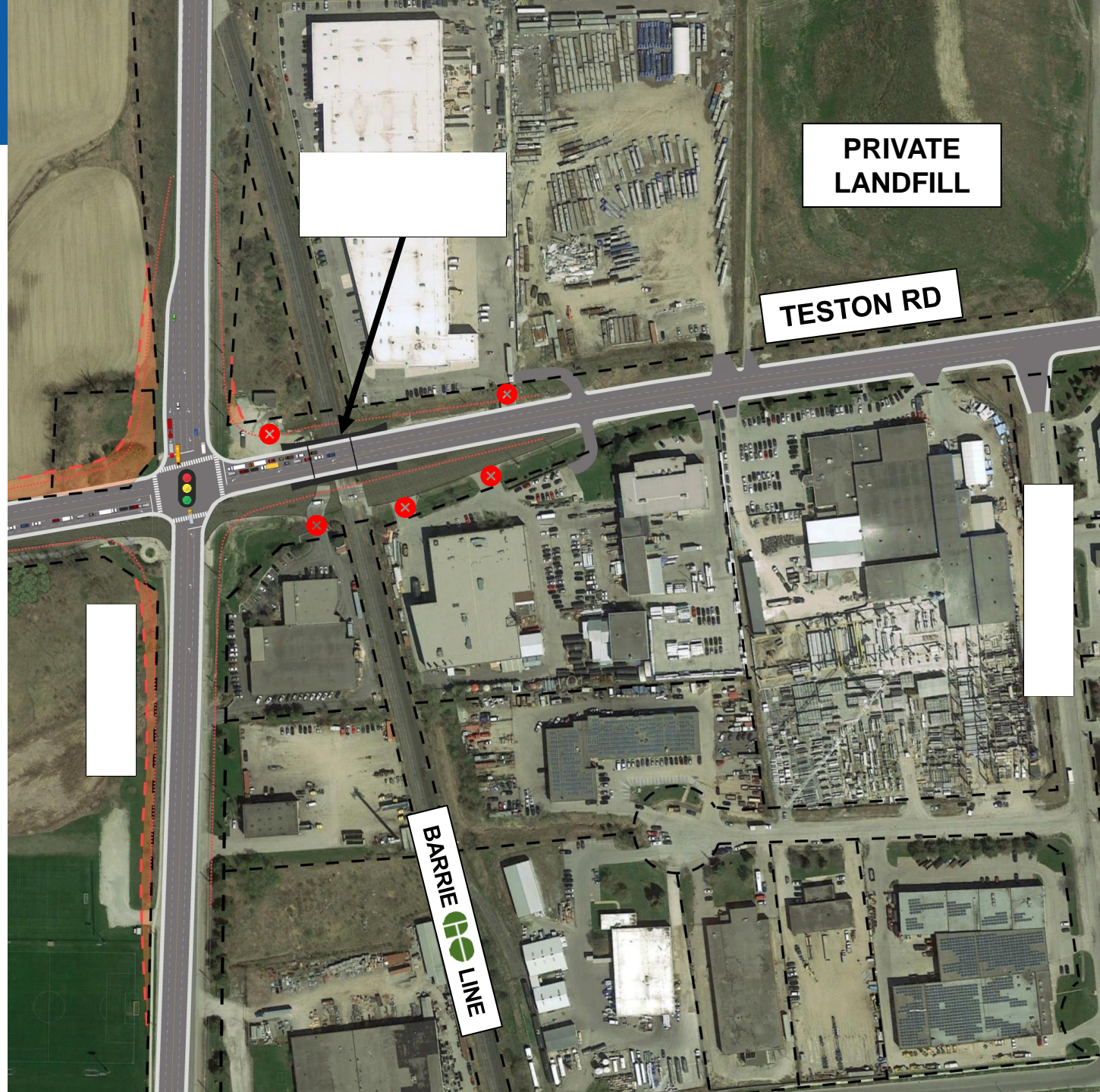
- Recommend an at-grade GO rail crossing – with improved Teston Road alignment (shift to north)
- Recommend long-term property protection for grade-separated GO rail crossing



	1. Existing Alignments / GO Rail Overpass	2. Shift Teston Road north / GO rail overpass	3. Shift Keele Street west / GO rail overpass	4. Shift Teston Road north, Keele Street west / GO rail overpass	5. Future do nothing
NATURAL ENVIRONMENT					
LAND USE / SOCIO-ECONOMIC ENVIRONMENT					
TRANSPORTATION					
ALTERNATIVE RANK					
EVALUATION RESULTS	NOT RECOMMENDED	CARRY FORWARD AS RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED

** Cultural Heritage Resources were not impacted by these alternatives; therefore, it was removed from the evaluation criteria.

Section 1 recommended long-term solution:
Alternative 2 —
Shift Teston Road north,
exiting Keele Street,
GO rail overpass



Section 2: Rodinea Road to Valley (Landfill Area)

- Given the extensive landfill-related infrastructure through the area, two cross section alternatives along with the do-nothing alternative were considered:
 1. Full width cross section
 2. Constrained cross section
 3. Future do nothing



Section 2: Constrained Cross Sections Plan



















Section 3: Valley Crossing

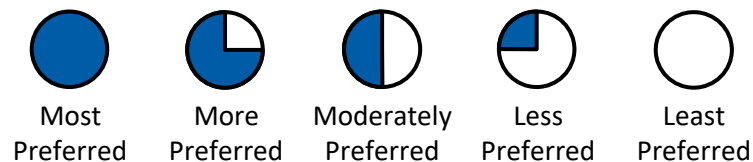
- Four valley crossing bridge alternatives were evaluated (including do nothing):
 - Single-span bridge
 - Double-span bridge
 - Triple-span bridge
 - Future do nothing



Section 3 Evaluation

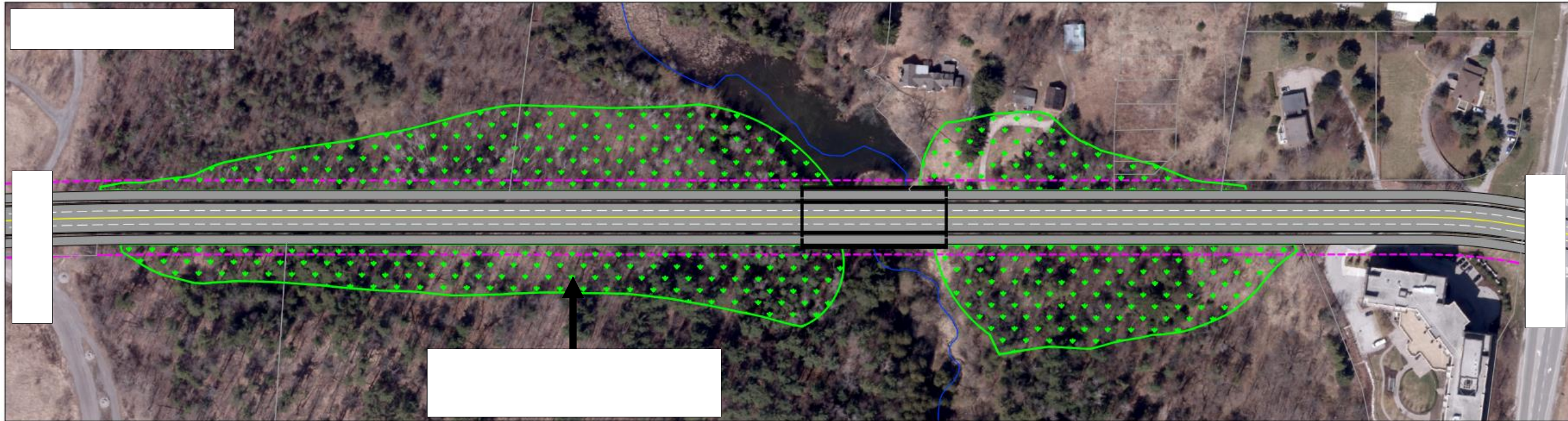
	1. Single-span bridge	2. Double-span bridge	3. Triple-span bridge	4. Future do nothing
NATURAL ENVIRONMENT				
LAND USE / SOCIO-ECONOMIC ENVIRONMENT				
TRANSPORTATION				
ALTERNATIVE RANK				
EVALUATION RESULTS	CARRY FORWARD AS RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED

** Cultural Heritage Resources were not impacted by these alternatives; therefore, it was removed from the evaluation criteria.



Section 3 Recommended Solution:

Alternative 1 — Single-Span Bridge



Section 4: Dufferin Street To Bathurst Street

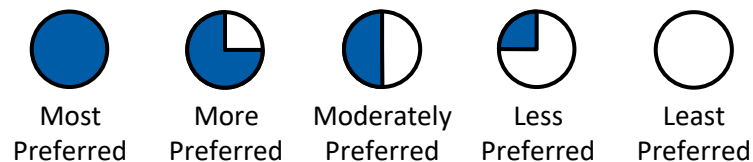
- Widening alternatives include:
 1. Widen equally on each side of the existing road
 2. Widen on the south side only
 3. Widen on the north side only
 4. Future do nothing – keep road as two lanes



Section 4 Evaluation

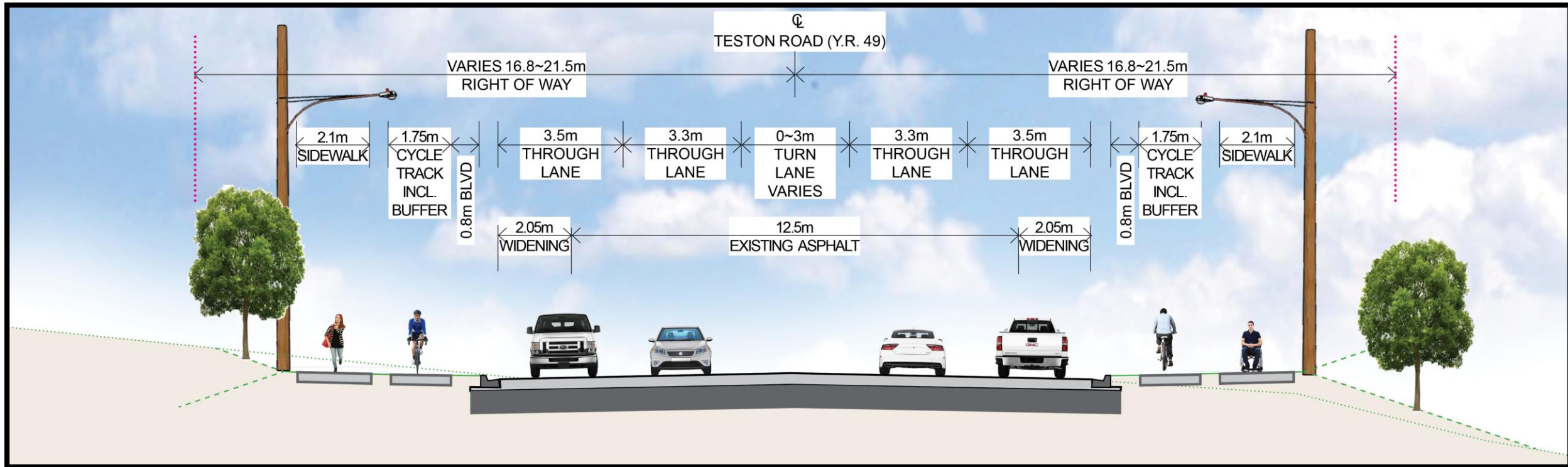
	1. Widen on both sides	2. Widen on the south	3. Widen on the north	4. Future do nothing
NATURAL ENVIRONMENT				
LAND USE / SOCIO-ECONOMIC ENVIRONMENT				
TRANSPORTATION				
ALTERNATIVE RANK				
EVALUATION RESULTS	CARRY FORWARD AS RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED	NOT RECOMMENDED

** Cultural Heritage Resources were not impacted by these alternatives; therefore, it was removed from the evaluation criteria.



Section 4 Recommended Solution:

Alternative 1- Widen Equally on Each Side of the Existing Road



Recommendation Summary

Section 1

- At-grade GO rail crossing – with improved Teston Road alignment (shift to north)
- Long-term property protection for grade separation

Section 2

- Constrained cross section used throughout this section with property protection for future full width cross section

Section 3

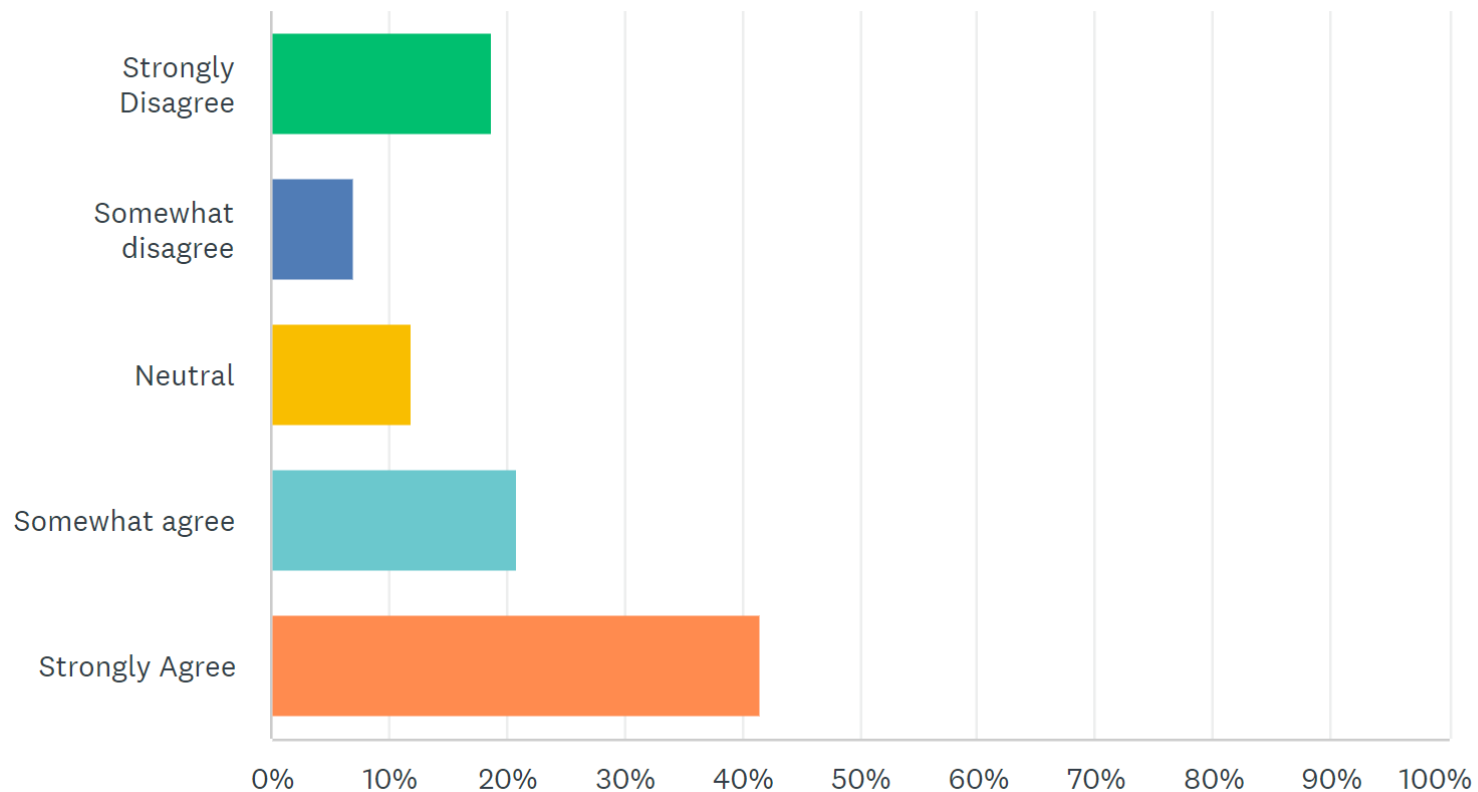
- Single-span bridge

Section 4

- Widen equally on both sides

Open House #3 - What We Heard

Do you agree with the results of the evaluation and the recommendation to proceed with each section alternative?



- Many comments received were in support of the recommendations and the project
- Main topics of concerns expressed included:
 - Transportation issues
 - Environmental impacts
 - Noise from new road
 - Process speed

Open House #4 Presentation

- Please watch the open house #4 presentation to learn about the preliminary design for improvements along the four sections of the project alignment, potential impacts and mitigation measures
- Video and presentation are available for viewing at www.York.ca/TestonRoad

THANK YOU



TESTON ROAD AREA TRANSPORTATION IMPROVEMENTS

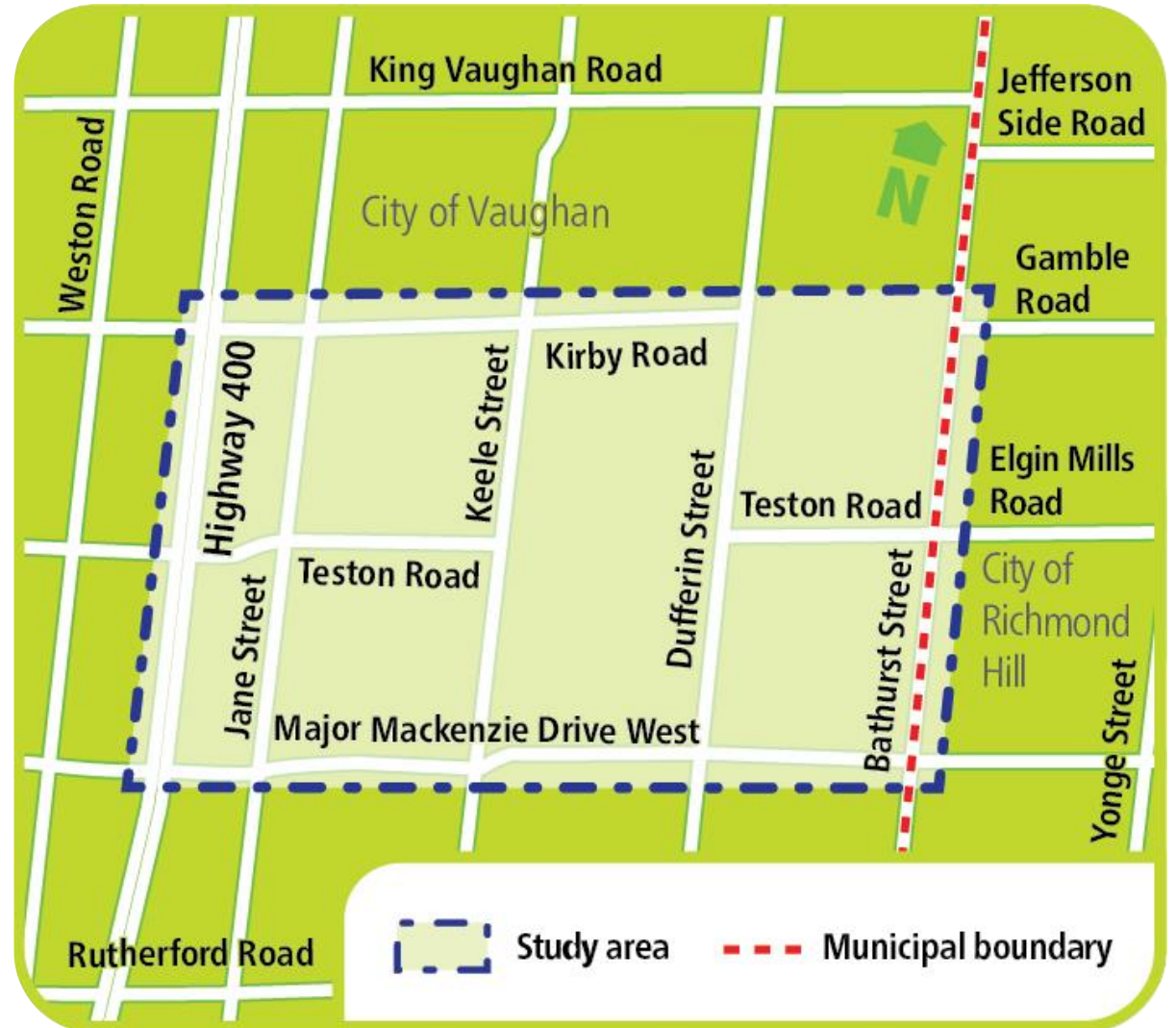
Individual Environmental Assessment

Online Public Open House #4

December 2023

Purpose of the Open House

- Present the preliminary design
- Provide results of the impact assessments and proposed mitigation measures
- Share next steps



Presentation Outline

- Project overview/schedule
- Present recommended design
- Preliminary impact assessment and mitigation measures
- Next steps



Your Feedback is Important

- Your participation is important to the study process
- Join the study mailing list to receive future study notices or submit comments and questions to transportation@york.ca
- Study updates can be found at www.york.ca/TestonRoad
- Please submit your comments on the open house materials by December 22, 2023
- Contact York Region at any time throughout the study to provide your feedback

Your Feedback is Important

Survey question
example



- A survey has been prepared to receive your feedback
- When you see the icon at the top of the slide, you may pause the presentation and answer the question(s)
- The survey can be accessed under the open house material heading at www.York.ca/TestonRoad
- Please complete the survey by December 22, 2023

Study Introduction

Survey questions
1 and 2



Survey available at york.ca/TestonRoad

- York Region is undertaking an Individual Environmental Assessment (IEA) to address transportation challenges and opportunities
- The study area falls within the City of Vaughan and borders the City of Richmond Hill
- The IEA started in spring 2020 and is expected to be complete in 2024

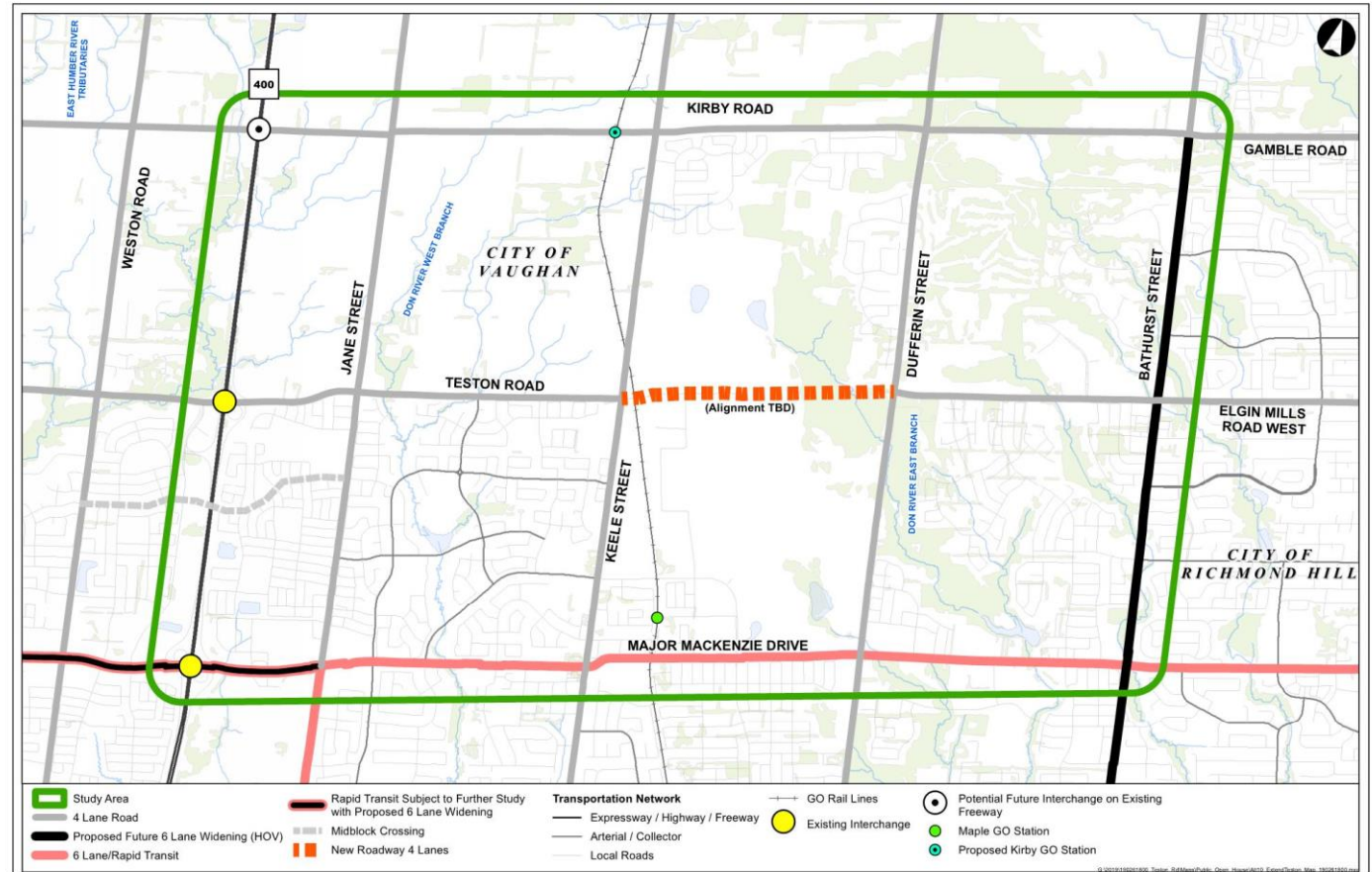


Study Schedule

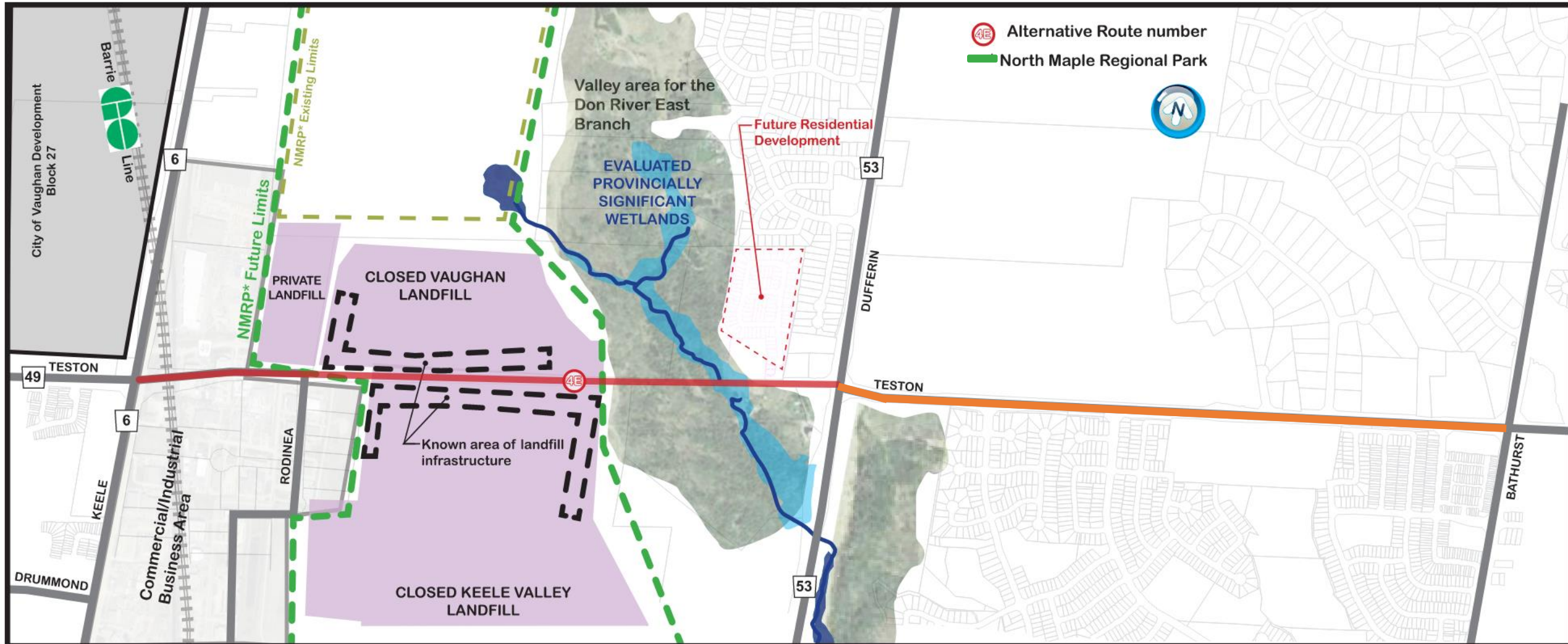
IEA key milestones	Completion date
Identification of problems and opportunities	Spring to fall 2020
Generation of alternatives to the undertaking	Winter to spring 2021
Open house #1	June 2021
Confirm preferred alternative to the undertaking	Summer 2021
Generation of alternative methods	Summer/fall 2021
Open house #2	Fall 2021
Select preferred alternative method	Fall 2021
Open house #3	Spring 2022
Preliminary design	Spring to fall 2022
Technical studies/impact assessments	Fall 2022 to summer 2023
Open house #4 – <u>We are here</u>	Fall 2023
Draft IEA Report (public and government review)	Winter 2024
Final IEA Report to Ministry of the Environment, Conservation and Parks (MECP)	Spring 2024

Preferred Alternative to the Undertaking

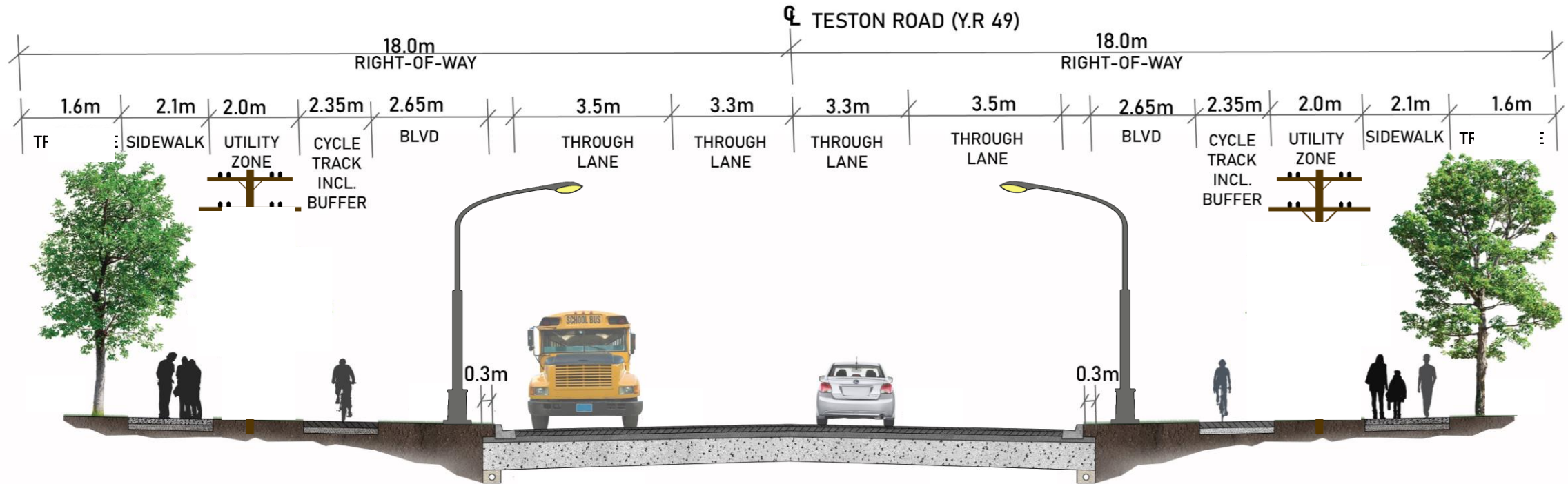
Following Open House #1, a new four-lane connection on Teston Road between Keele Street and Dufferin Street with active transportation and potential for transit service (alternative 4) was confirmed as the preferred alternative to the undertaking.



Preferred Alternative Alignment



Future Full Width Cross Section (For All Sections)



Project Sections



Section 1: Keele Street to Rodinea Road (GO Crossing)

Section 2: Rodinea Road to valley (Landfill Area)

Section 3: Valley crossing

Section 4: Dufferin Street to Bathurst Street

Summary of Preferred Design Alternatives from Open House #3

Alignment section	Preferred design alternative
Section 1: Keele Street to Rodinea Road (GO Rail Crossing)	<ul style="list-style-type: none">• Widened four-lane roadway• At-grade GO Rail Crossing with improved Teston Road alignment (shift to north)• Long-term property protection for GO Rail/Teston Road grade separation
Section 2: Rodinea Road to valley (Landfill Area)	<ul style="list-style-type: none">• New constrained four-lane cross section with property protection for future full width cross section
Section 3: Valley crossing	<ul style="list-style-type: none">• New constrained four-lane cross section with property protection for future full width cross section• Single-span bridge
Section 4: Dufferin Street to Bathurst Street	<ul style="list-style-type: none">• Widen equally on both sides to four-lanes

Section 1: Design Considerations

- GO Rail Crossing – proximity to the Teston Road and Keele Street intersection and at-grade versus grade separation
- Planned Block 27 development within the northwest quadrant of Keele Street and Teston Road
- Industrial facilities and accesses east of Keele Street
- Natural areas and existing residential area in the southwest quadrant

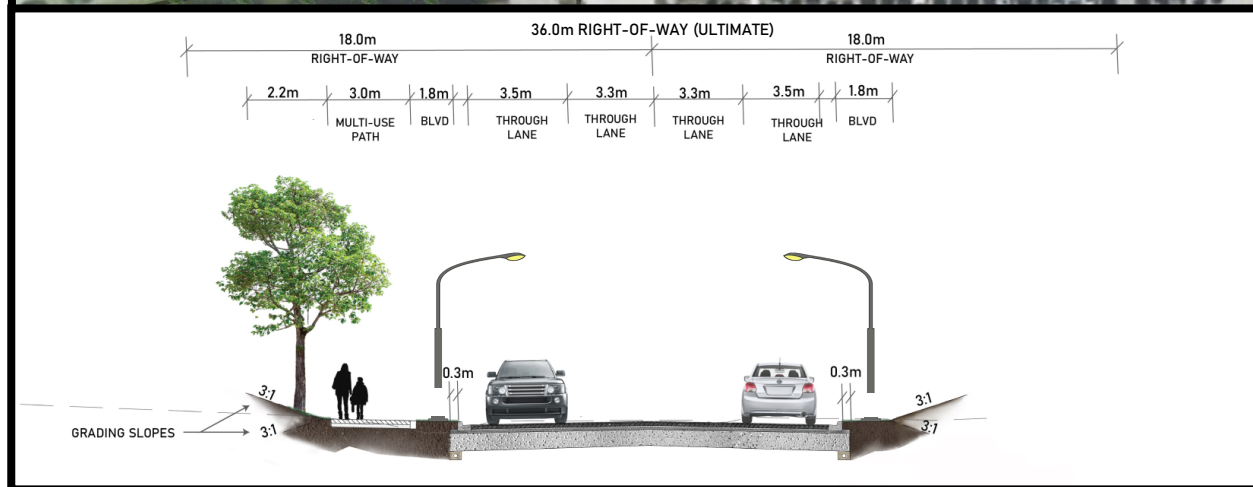
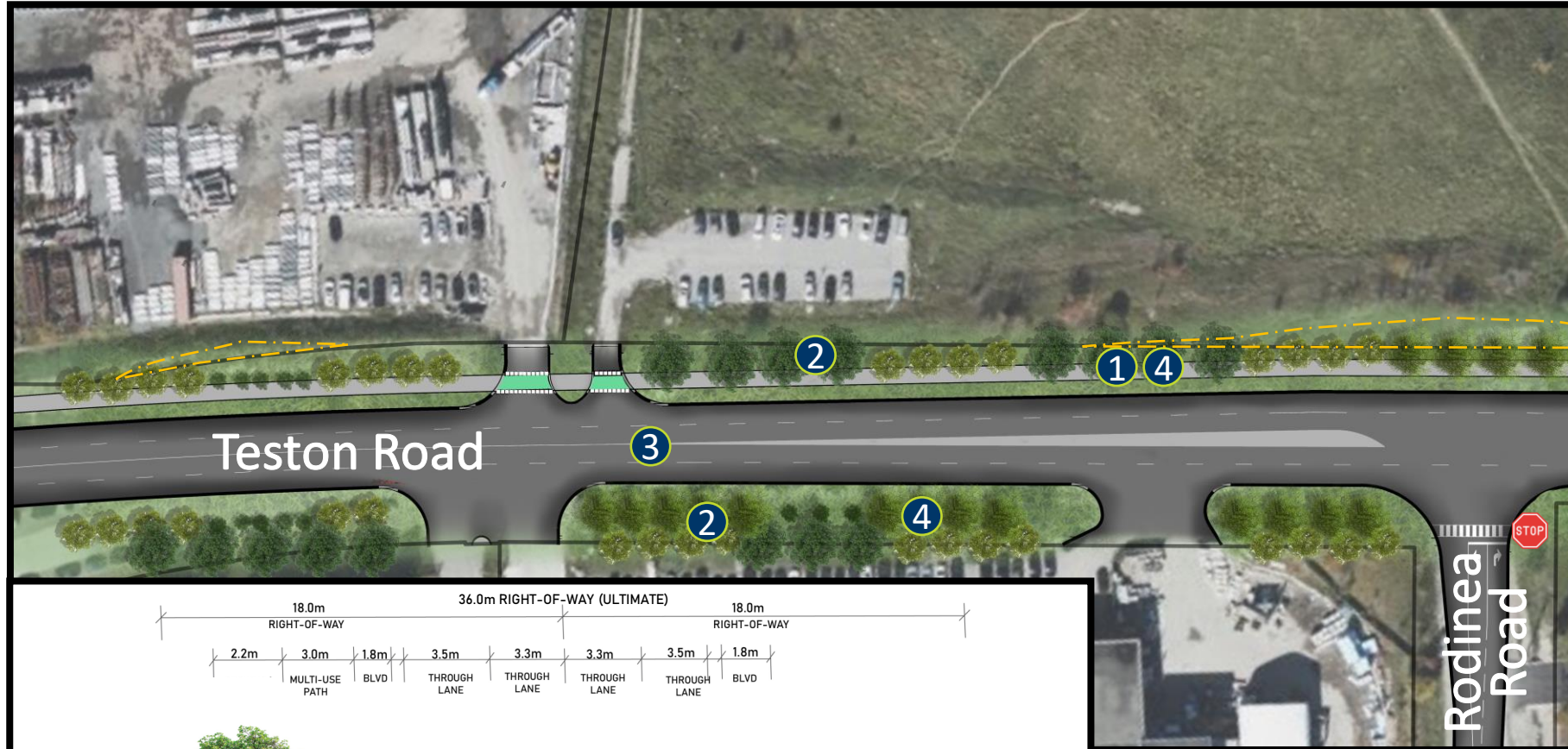
Section 1: Preliminary Design



Section 1 features

- ① At-grade crossing (with safety measures)
 - ② Multi-use pathway
 - ③ Street plantings
 - ④ Widened to four lanes
 - ⑤ Stormwater retention
 - ⑥ Future sidewalk and cycle track
- Potential property required
- Potential grading limits on private property

Section 1 (cont.): Preliminary Design



Section 1 Features

- ① Multi-use pathway
 - ② Street plantings
 - ③ Widened to four lanes
 - ④ Future sidewalk and cycle track
- Potential grading limits on private property

Survey Questions 3 and 4 ?

Survey available at york.ca/TestonRoad

Section 1: Potential Impacts and Mitigation Measures

- Limited natural environment impacts associated with footprint increases
- Stormwater flows directed to the stormwater retention area in southwest quadrant of Teston Road and Keele Street via ditching/culverts
- At-grade GO rail crossing fits within existing right-of-way. Future grade separation requires easements/property for grading
- Some property accesses impacted by grade separation but can be accommodated in alternative ways
- Future GO rail grade separation may require additional noise reduction measures for residential properties in the southwest quadrant of Teston Road and Keele Street

Section 2: Design Considerations

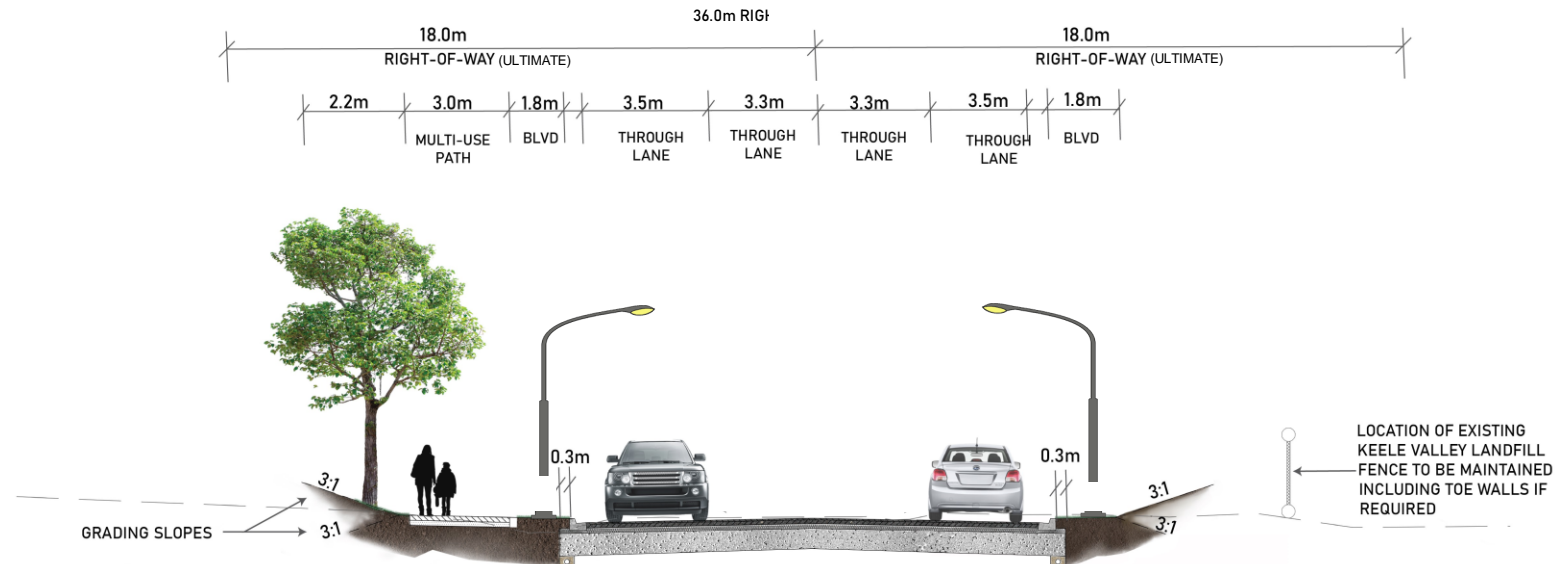
- Avoid complex landfill infrastructure present throughout this section
- Provide connectivity for active transportation users, including links to planned trails and potential Teston Road crossings
- Maintain existing access to landfills from Teston Road
- Prepare for future public access to North Maple Regional Park

Section 2: Preliminary Design



Section 2 features

- ① Multi-use pathway
- ② Plantings/revegetation
- ③ New four-lane road
- ④ Future sidewalk and cycle track
- ⑤ Landfill access
- ⑥ Existing gas flare facility
- ⑦ Future trail connection (potential)
- Potential grading limits on private property



Section 2 (cont.): Preliminary Design



Section 2 features

- ① Multi-use pathway
- ② Street plantings
- ③ New four-lane road
- ④ Future sidewalk and cycle track
- ⑤ Future trails (potential)
- ⑥ Plantings/revegetation
- ⑦ Wildlife fencing
- ⑧ Landfill access

Survey questions
5 and 6



Survey available at york.ca/TestonRoad

Section 2: Potential Impacts and Mitigation Measures

- Natural environment impacts associated with new roadway footprint, however, use of the existing access road reduces overall impact
 - Parklands/landfills contain species at risk such as the grassland bird habitat
- Constrained cross section fits within existing York Region right-of-way, however, protection for future 36-metre right-of-way is required
- Stormwater management split between flowing westerly to Section 1 facilities and easterly to Section 3 facilities
- Consideration to be given to existing landfill groundwater plume (concentrated form of liquid contaminants that is formed when substances are released to groundwater from a source facility) and isolation from impact of road salt application

Section 3: Design Considerations

- Provide connectivity for active transportation users, including linkages to planned trails and potential Teston Road crossings
- Deep valley requiring elevated roadway
- Spanning the existing Don River east branch tributary
- Access to existing residential properties at Dufferin Street and future developments
- Existing constructed dam, pond in valley
- Natural environmental impacts, including tree removal and habitat fragmentation

Section 3: Preliminary Design



Section 3 features

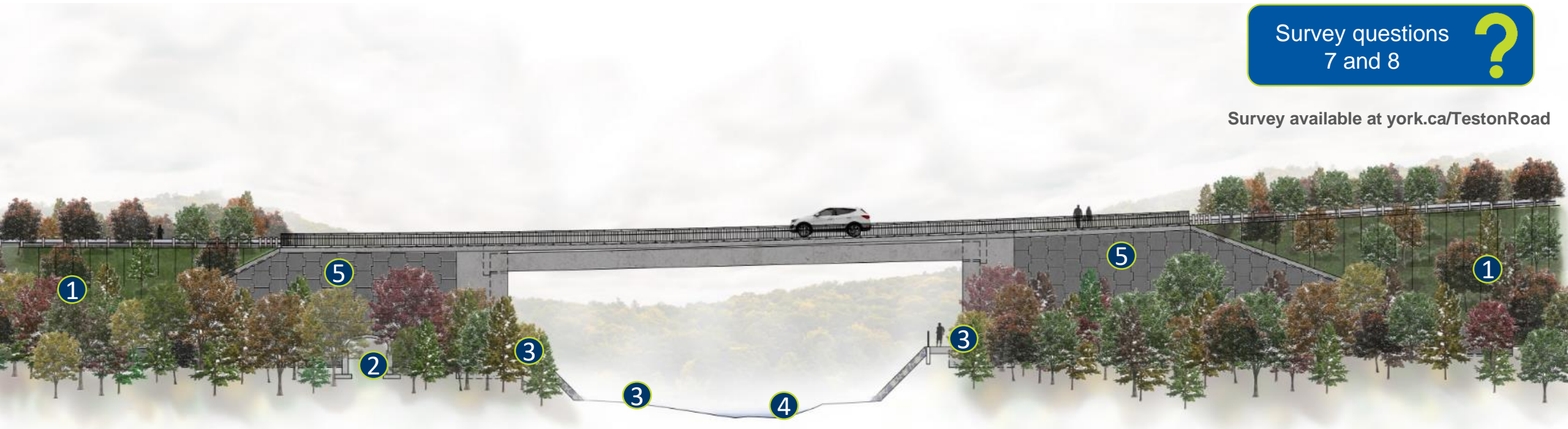
- ① Multi-use pathway
- ② Bridge
- ③ New four-lane road
- ④ Future sidewalk and cycle track
- ⑤ Future trail (potential)
- ⑥ Plantings/revegetation
- ⑦ Wildlife fencing
- ⑧ New municipal road
- ⑨ Wildlife crossing under roadway (potential)
- Existing pond/watercourse
- - - Potential property required
- - - Potential grading limits on private property

Section 3: Preliminary Design

Survey questions
7 and 8

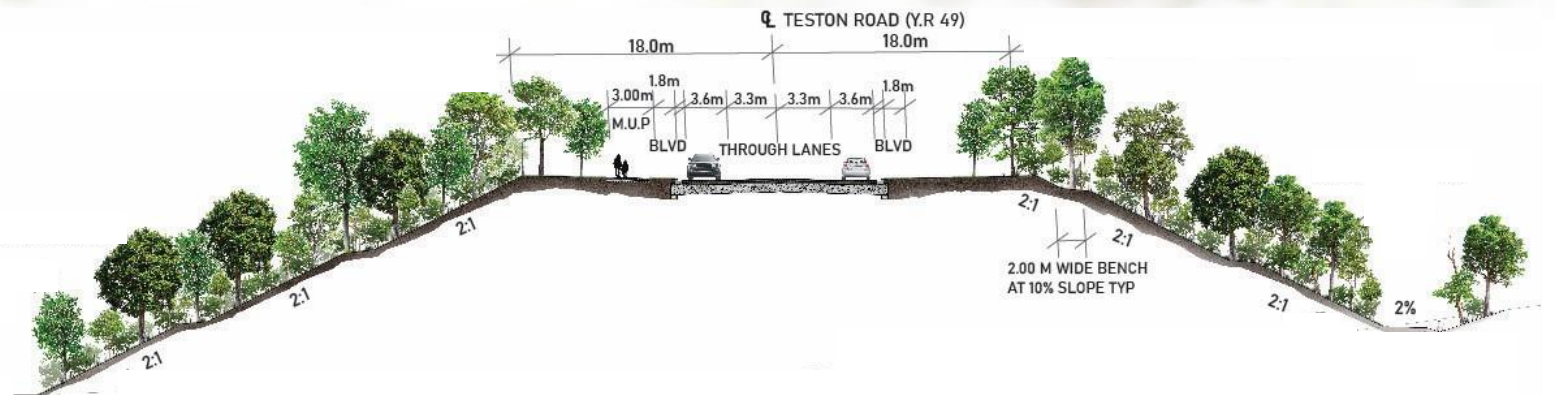


Survey available at york.ca/TestonRoad



Section 3 features

- ① Revegetated embankments
- ② Wildlife passage (Potential)
- ③ Potential trail location
- ④ Watercourse
- ⑤ Vertical retaining wall

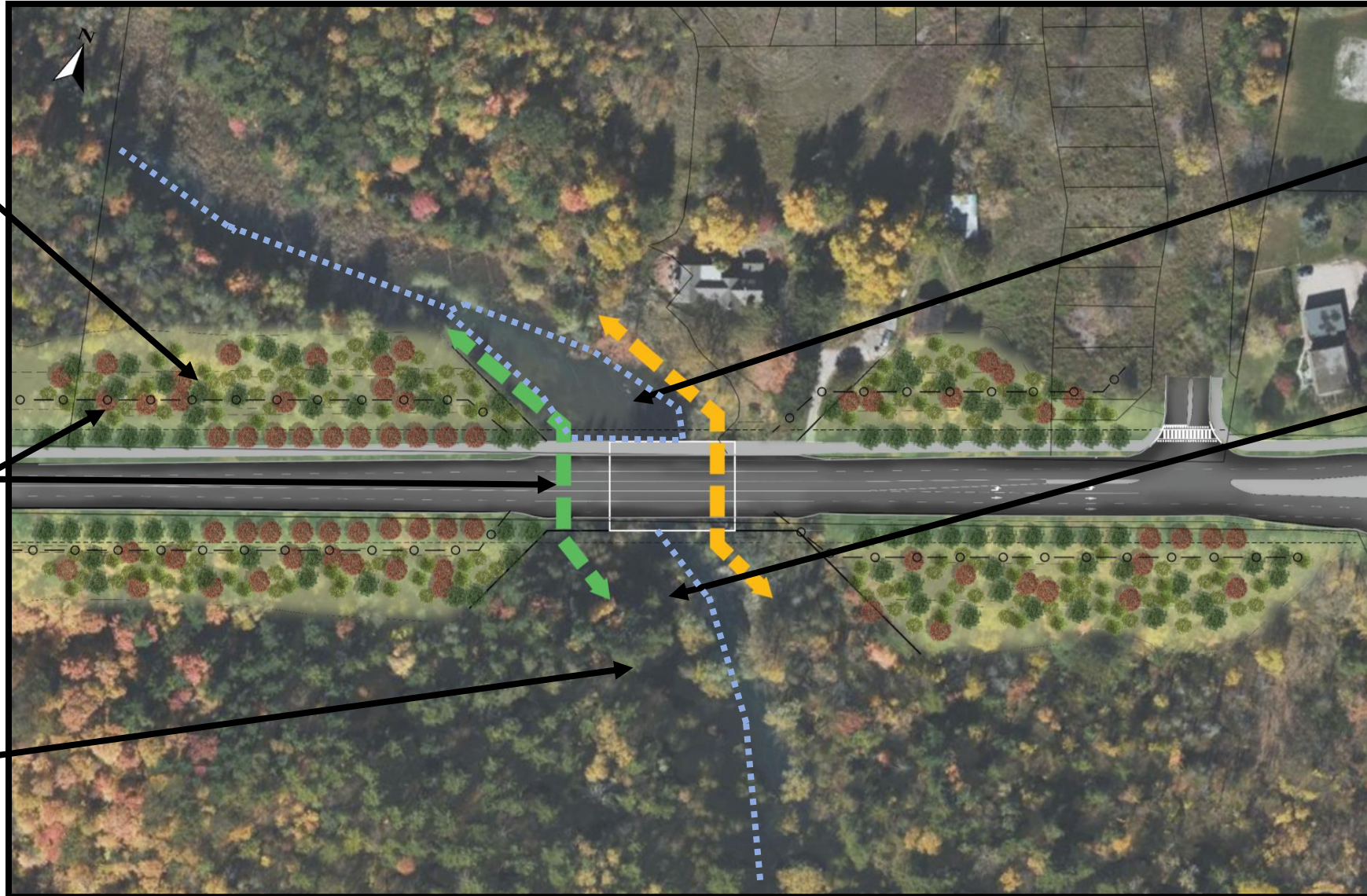


Section 3: Potential Impacts and Mitigation Measures

Recommendation #1:
Revegetation of the embanks reduces the permanent footprint impacts of the project.

Recommendation #2:
Wildlife fencing and wildlife crossings prevent roadway crossings and help direct wildlife through the valley.

Recommendation #3:
Removal/management of invasive species in the valley helps promote native species.



Recommendation #4:
Conversion of the existing pond to a natural wetland improves habitat quality for fish and amphibians.

Recommendation #5:
Plantings along the stream banks can improve conditions and habitat quality as well as water quality.

Recommendation #6:
Plantings and habitat creation outside of the right-of-way to compensate for footprint impacts and impacts to specific habitat types (for example, bat habitat).

Section 3: Potential Impacts and Mitigation Measures

- Total permanent footprint impacts of 2.2 hectares and an additional 1.8 hectares temporarily impacted by construction that will be restored
- The valley likely contains species at risk including bat habitats (suitable habitat is present, acoustic surveys to be completed during detail design). Offsetting plans typically include installation of artificial habitat structures (bat boxes), planting plans, monitoring and reporting
- Stormwater management being addressed by storage/treatment facilities under the roadway and outlets to stream

Section 4: Design Considerations

- Reduce impacts to property
- Reduce impacts to existing culvert
- Minimize utility relocations (hydro poles on the south side)
- Consider traffic signals at each intersection along Teston Road
- Improve/use existing stormwater management infrastructure
- Connect active transportation infrastructure at Bathurst Street to infrastructure to the east

Section 4: Preliminary Design

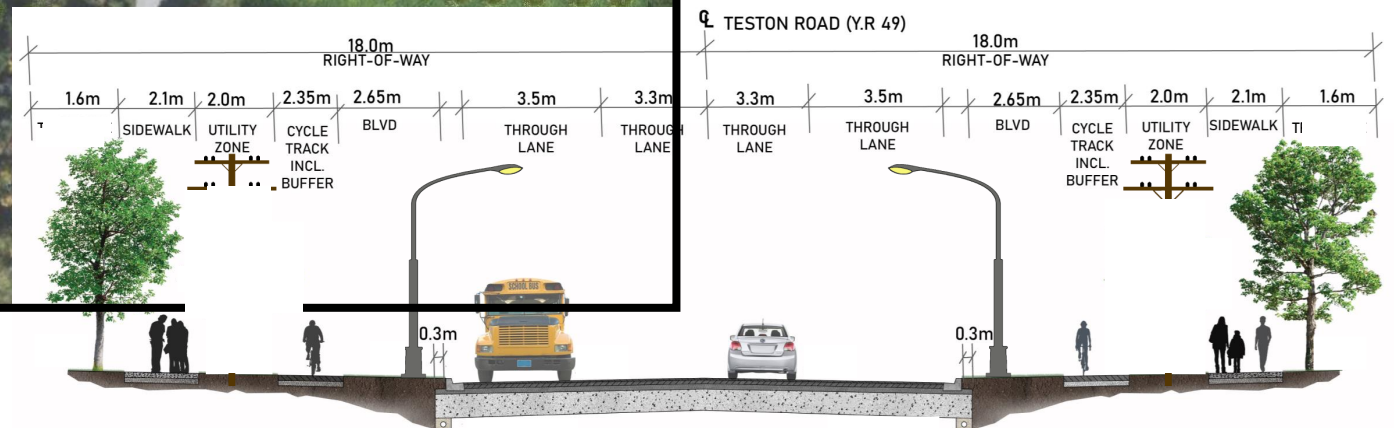


Section 4 Features

- ① Multi-use pathway
- ② Sidewalk and cycle track
- ③ Street plantings
- ④ Widened to four lanes

--- Potential property required

- - - Potential grading limits on private property



Section 4 (cont.): Preliminary Design



Section 4 Features

- ① Sidewalk and cycle track
- ② Bus stop
- ③ Street plantings
- ④ Widened to four lanes
- - - Potential grading limits on private property

Section 4 (cont.): Preliminary Design



Section 4 features

- ① Sidewalk and cycle track
- ② Street plantings
- ③ Widened to four lanes
- ④ Transition to on-street bike lanes
- Existing watercourse

Survey questions 9 and 10 ?

Survey Available at york.ca/TestonRoad

Section 4: Potential Impacts and Mitigation Measures

- Limited natural environmental impacts due to the increase in the width of the roadway occurring within the existing right-of-way
- Limited grading requirements outside of right-of-way along north side of Teston Road from Dufferin Street to Lady Fenytrose Avenue
- Proposed stormwater management through upgrades to existing facilities along this section
 - Coordination with the City of Vaughan required to integrate with existing stormwater management
- Limited property acquisition required
- Noise lessening measures are not required

Air Quality and Climate Change

Survey questions
11 and 12



Survey Available at york.ca/TestonRoad

Air quality:

- Maximum combined concentrations below guidelines, except where background concentrations exceeded the guideline
- Overall contribution from the roadway emissions to the combined concentrations was small. Extra measures are not warranted

Climate change:

- Recommendations divided into design and policy categories (for example, monitoring and inspection) measures
- Potential greenhouse gas mitigation measures proposed for: construction equipment emissions, maintenance equipment emissions and embodied carbon in materials

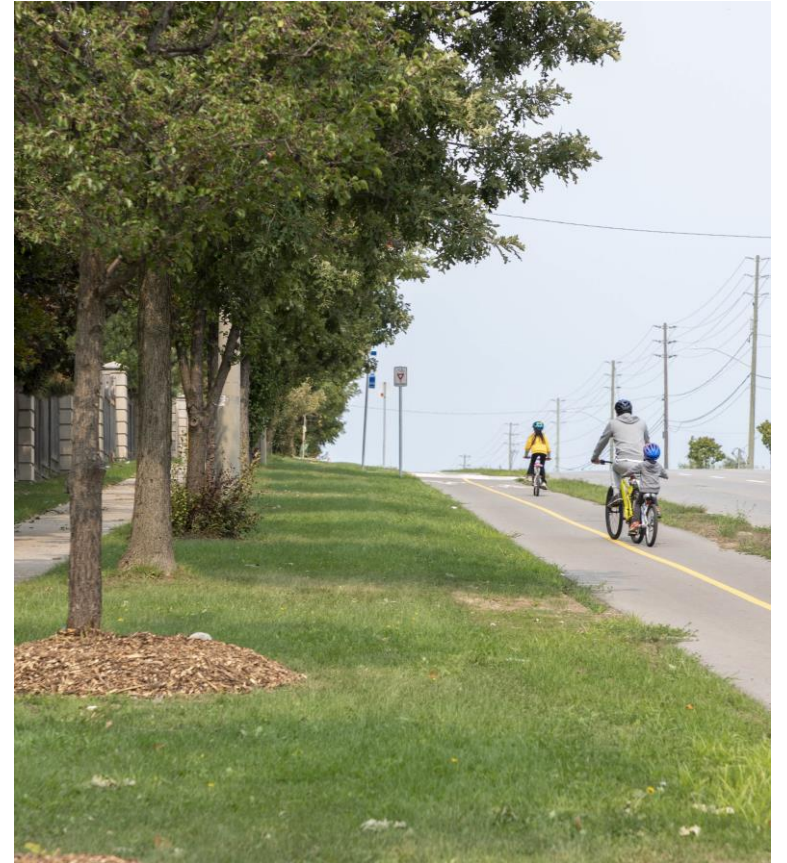
Next Steps

- Review feedback received from Open House #4
- Subject to feedback received and further review, develop the Individual Environmental Assessment Report, documenting the process and seek approval for the project from the Minister of the Environment Conservation and Parks (MECP)

Survey Question
13



Survey Available at [York.ca/TestonRoad](https://york.ca/TestonRoad)



Your Feedback is Important

Survey Questions
14 & 15



Survey Available at York.ca/TestonRoad

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- Study updates can be found at www.york.ca/TestonRoad
- Please submit your comments on the open house materials by December 22, 2023
- Contact York Region at any time throughout the study to provide your feedback

Thank You

York Region, Public Works

Phone: 1-877-464-9675 ext. 75000

TTY: 1-866-512-6228

Email: transportation@york.ca



APPENDIX C: GENERAL COMMENTS RECEIVED

Teston Road IEA – Comment Tracking Table

Date	Name/Title/Organization	Type	Comment Received	Action/Response
Public				
12/12/23	Unnamed Resident	Email	<p>Good afternoon,</p> <p>I am writing again due the open house #4 for the opening of Teston Road.</p> <p>The area that has been proposed to be open will impact land with trees and natural forest. Additionally, it will create more traffic on Teston that it is really busy due the bottle neck that was created by the curve constructed when Teston approaches Bathurst.</p> <p>Kirby will be opened and there is not need to open Teston due to the environmental impact that the new road will cause in the area.</p> <p>Thank you for your consideration to this matter.</p>	<p>Traffic analysis shows that there is a projected need for additional roadway capacity within the study area even after accounting for the future extension and widening of Kirby Road to four lanes through the study area and also accounting for additional east-west peak period trips being carried in future by transit.</p> <p>After consideration of a broad range of alternatives and environmental considerations, the extension of a four-lane Teston Road between Keele Street and Dufferin Street and the widening of Teston Road to four lanes between Dufferin Street and Bathurst Street are recommended as future improvements as part of the Teston Road Individual Environmental Assessment (IEA) Study. Other recommended improvements include new facilities for active transportation, stormwater management and landscaping throughout.</p> <p>The above improvements will come with some potential effects including impacts to trees and natural forest such as in the area of the Don River Tributary valley. The recommended design in this area includes measures to reduce the amount of impact within the valley in the form of extensive vertical retaining walls and includes replacement plantings on the roadway embankments and other new plantings and revegetation nearby. The IEA Report will include more details on these recommendations and will be made publicly available for review and further comment.</p>
12/05/23	Peter Pasioka	Comment on Social Media	<p>Are you connecting Kirby to Gamble and Teston to Teston?</p>	<p>Thank you for your question. York Region is undertaking a study of the Teston Road Area. The preferred design includes the extension of Teston Road between Keele Street and Dufferin Street as a new roadway connection and the widening of Teston Road from two to four lanes. More information can be found at York.ca/TestonRoad</p> <p>The Kirby Road extension between Dufferin Street and Bathurst Street is being undertaken by the City of Vaughan</p>
Agency				
12/22/2023	Alexander Wilkes	Email	<p>Please be advised that I have now been retained by Dr. David Wilkes and Mrs. Angela Wilkes, the owners of the above referenced Property, to respond to the “Proposed Teston Road Area Improvements”, identified as City of Vaughan Project Number 99816 (the “Project”). I am further writing on my own behalf, as the son of David and Angela, and as an individual who has lived at the Property for over 20 years now.</p> <p>We have reviewed the online materials related to the Project, including but not limited to the PDF documents entitled “Project Summary Presentation”, and “Open House #4 Presentation”. The proposed expansion of Teston Road, particularly the expansion of Teston Road “Section 4” into a four lane road, is deeply concerning, and will have serious adverse effects on the ability to use safely the Property in its current form.</p> <p>David and Angela have owned the Property for over 20 years now, having purchased it in March of 2002. Since that time, as the</p>	

Note: Comments are reproduced verbatim including typos and/or grammar errors

Teston Road IEA – Comment Tracking Table

Date	Name/Title/Organization	Type	Comment Received	Action/Response
			<p>region has grown in population density, safe access to the Property as a rural residential home has become increasingly precarious. Numerous issues with access to the Property already exist, including but not limited to:</p> <ol style="list-style-type: none"> 1. When travelling west on Elgin Mills to reach the Property, there are frequent instances where individuals use the westbound right turn lane at the Bathurst intersection as a through lane, necessitating evasive maneuvers when attempting to make a right turn into the Property; December 22, 2023 Alexander D. Wilkes 800 Teston Road Maple, Ontario L6A1E9 Tel: 647.544.0512 Email: alexander@wilkeslaw.ca 2. Many years ago, when the City of Vaughan wished to add a island in the middle of Teston near the intersection, negotiations took place between my parents and the City, resulting in a drop in the island near the intersection to allow left turns into the Property and leaving the Property, so we would not lose our ability to access the Property. Given our proximity to the intersection, further lane increases in this roadway will almost certainly make exiting the Property via a left turn impossible, and further increase the hazard of the right turn leaving the Property <p>Over the past many years, we have corresponded with the City and have inquired as to the possibility of rezoning the Property for more appropriate uses. Discussions have included allowing for an additional entry point to the Property on Bathurst, and a potential rezoning of the Property to allow for a more appropriate use of the Property (proposals have included substantially more environmentally conscious townhomes compared to the current many decade old property, and/or a retirement residence). Thus far, all proposals have been met with a response from the City that there is no intention to allow the Property to be rezoned beyond Rural Residential.</p> <p>If this Project is to move forward, we strongly encourage the City (in collaboration with the Province, if need be) to rethink its position in relation to the zoning of this Property. If there is seriousness about minimizing the Environmental Impact of this zone, the obviously appropriate response is to allow the Property to be more efficiently and effectively utilized, in a modern way, more respectful of the scarcity of land for housing Vaughan's growing population in the region.</p> <p>Please contact me by email at alexander@wilkeslaw.ca or by telephone at my office at 647-544- 0512 to discuss this Project moving forward in a way that is maximally beneficial to all parties involved.</p>	

Note: Comments are reproduced verbatim including typos and/or grammar errors

Teston Road IEA – Comment Tracking Table

Date	Name/Title/Organization	Type	Comment Received	Action/Response
12/19/23	Bousfields	Email	<p>Yours very truly,</p> <p>We are the planning consultants to Ferrara Glade Investments Inc. (the “Owner”), owner of 2270 and 2400 Teston Road (the “subject lands”) which form part of Block 27, a 400-hectare tract of land bounded by Keele Street, Teston Road, Jane Street and Kirby Road.</p> <p>We understand that York Region (the “Region”) is undertaking an Individual Environmental Assessment Study to evaluate, among other things, the realignment of Teston Road along the frontage of the subject lands.</p> <p>We have reviewed the preliminary design and have some concerns and questions:</p> <ol style="list-style-type: none"> 1. As you may be aware, the Owner is collectively engaged in the Block Planning process with the Block 27 Landowners’ Group, with the first submission taking place in August 2022 and a second submission in November 2023. The Owner has already attended a Pre-Application Consultation in anticipation of filing a Draft Plan of Subdivision in early 2024. Any additional loss of land through the IEA process would unfairly impact the design, density and ultimate unit yield on the subject lands. It may also impact timing of the delivery of the development. Further, based on the grading, it may impact construction methodology. 2. As you are aware the subject lands are located within the Urban Area, designated Low-Rise Mixed-Use, and are intended for higher density residential uses and supporting/accessory commercial uses. In this regard, it is our opinion that the Region should seek to limit the amount of land needed from the subject lands to accommodate the Teston Road re-alignment. Furthermore, based on conversations with the Owner and their consulting team, in order to build out the subject lands as planned in advance of completion of the Teston Road realignment, the Owner may be required to construct a parking level 2 partly or entirely above grade, which would remain exposed until the road is constructed and the grading is raised to bury the underground parking. Additional work and costs, such as installing drainage layers, may be required, and some complications in the design of the building and slab edge drawing could occur. In this regard, we respectfully request that the road be pushed as far south as possible in order to allow for the subject lands to be developed for their planned uses and accommodate housing for this growing City and Region. 3. Based on our review of the subject lands and surrounding streets, there are significant grade changes surrounding 	

Note: Comments are reproduced verbatim including typos and/or grammar errors

Teston Road IEA – Comment Tracking Table

Date	Name/Title/Organization	Type	Comment Received	Action/Response
			<p>the subject lands. In this regard, has the preliminary design accommodated for the grade change within the planned rightof-way or is additional lands needed? In our opinion, the design should illustrate all lands needed to accommodate grade changes as well as any technical requirements (i.e. daylight triangles, utilities, etc.).</p> <p>4. It is unclear how the Region will be obtaining the additional lands (especially from the subject lands) required to accommodate the realignment. In this regard, we respectfully request the Region to confirm how this additional land will be acquired. We also request that the IEA process be expedited in order to allow for the planned development of the subject lands to proceed as planned (see item 1 above).</p> <p>Thank you for the opportunity to provide input. As outlined above, we respectfully request a meeting and/or a response in order to resolve our comments and questions in order to allow the IEA process to proceed and, hopefully, be expedited quickly. In this regard, please contact the undersigned at 416-418-5422 or via e-mail at dfalletta@bousfields.ca to coordinate this meeting.</p>	

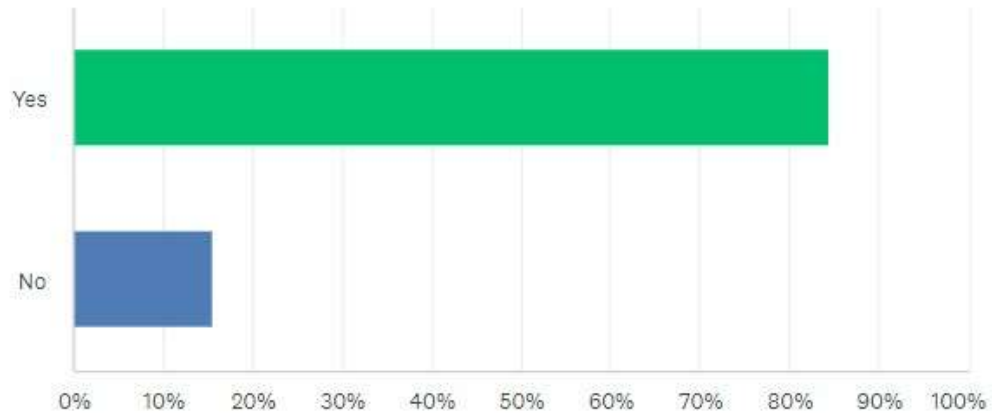
Note: Comments are reproduced verbatim including typos and/or grammar errors

APPENDIX D: SURVEY RESULTS

Q1

Do you live in the study area?

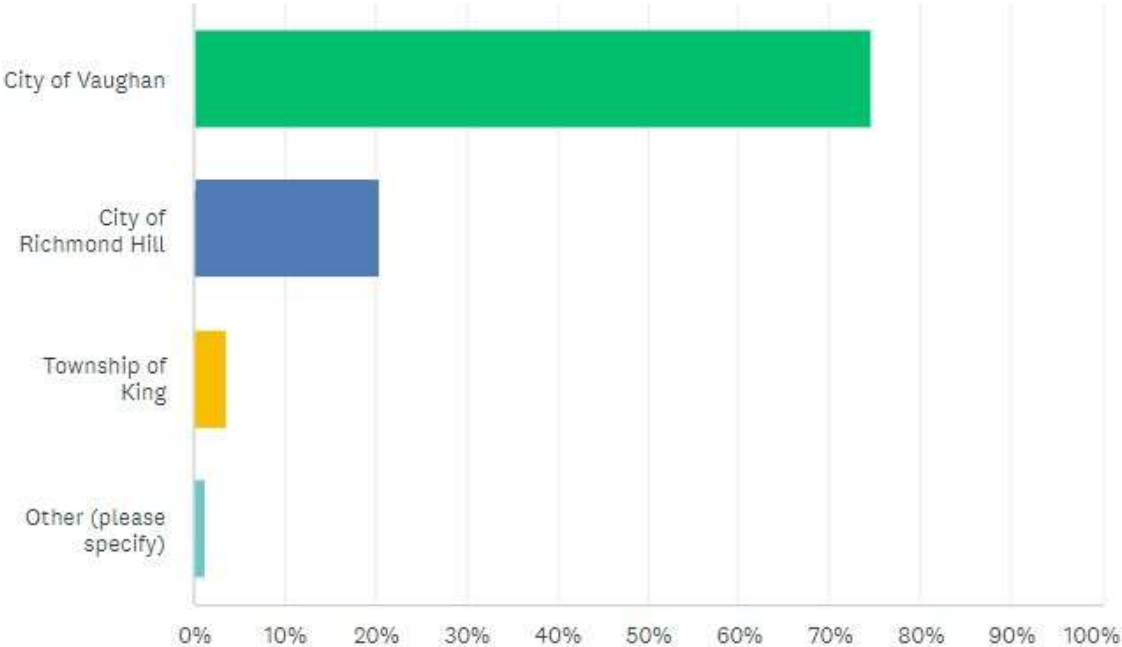
Answered: 83 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	84.34%	70
No	15.66%	13
TOTAL		83

Which municipality do you live in?

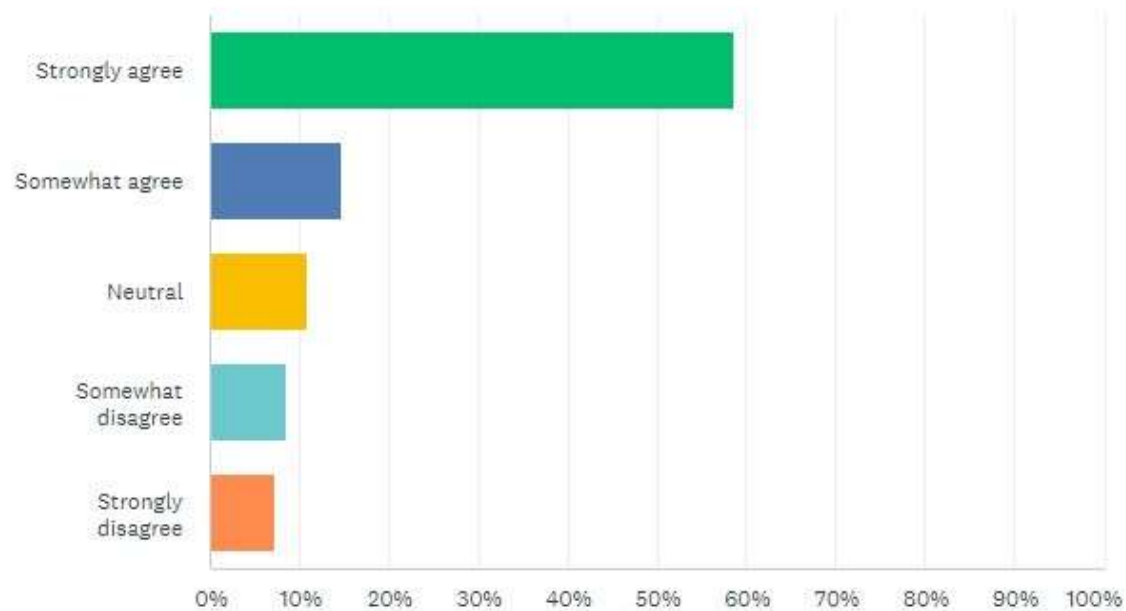
Answered: 83 Skipped: 0



ANSWER CHOICES	RESPONSES	
City of Vaughan	74.70%	62
City of Richmond Hill	20.48%	17
Township of King	3.61%	3
Other (please specify)	Responses 1.20%	1
TOTAL		83

Do you agree/disagree with the preliminary design recommendations for Section 1 (for example, northerly shift of Teston Road, road widened to four lanes, the road crossing level with the GO rail tracks, multi-use pathway on the north side of Teston Road)?

Answered: 82 Skipped: 1



ANSWER CHOICES	RESPONSES	
Strongly agree	58.54%	48
Somewhat agree	14.63%	12
Neutral	10.98%	9
Somewhat disagree	8.54%	7
Strongly disagree	7.32%	6
TOTAL		82

Q.4 Do you have any suggestions for improvements to the Section 1 preliminary design? Please enter them in the text box below:

Traffic lights at Teston and Quail Run Blvd

Consider making Go rail tracks not at street level

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

I only somewhat agree with the design from question 3 above because I think that a permanent solution to the railway crossing should be part of the design from the get go and not postpone it to a future time. The reason for this is that the back-up of traffic during the times when the trains will be crossing the railway crossing will cause traffic challenges that will encroach into the Keele & Teston road intersections. This is already evident today when the trains are crossing and given the limited use of this road at present to just the local traffic. Once Teston road becomes a through fare road, the traffic will increase causing challenges to interfere with the Keele and Teston road intersection itself. Hence, my recommendation would be to have the GO trains overpass be part of the initial design of the implementation. While I do understand that this will increase the timelines of completion, it would be a better option for the situation and will be more cost effective in the long run. In fact, I would recommend that a over pass be built that goes over Keele street and the Go Train tracks would be a more ideal solution for the development of this study area.

Grade separation should be provided for smooth traffic flow, the goal of this whole project. Also, the at grade crossing is very close to Keele which might cause eastbound traffic to block the Teston/Keele intersection when a train is passing. Discuss with province/metrolinx to make this happen!

no

not sure if 19th Ave. can be directly reach to Dufferin st.?

This work should start asap to lower traffic congestion.

Teston improvements may encourage more traffic to GO station via rodinea road. The proposed at grade crossing may result in standstill traffic building up on the west side of Keele given proximity of railway to the Keele/Teston intersection. Would it make more sense to do an overpass so as to not obstruct traffic when train is crossing?

Not a fan of the investment for this project without performing the grade separation. Concern is with the expansion of Barrie line trains and this new corridor, there will be lengthy wait times for stopped traffic. Please consider this investment the perfect opportunity for grade separation before the new road connection is opened and traffic is used to this configuration. There will be significant upset if the project has to be reopened in 5 years after completion to perform the grade separation at a much higher cost than today's dollars.

There has been a lot of well skilled resources which went into this study regarding Section 1's preliminary design. With the rules and regulations that conforms to the Municipality of York, there should be no need for further improvements to this design for Section 1.

Add multiple routes to Teston

please provide timelines for the execution of these much needed improvements , our family strongly support these improvements

All existing roads and railroads crossings nearby are sunken with time, and create inconvenient situation when there is need to nearly stop to slowly cross them. Such situation will promote traffic jams, which eliminate benefits of this connection. Building an elevated above rail road crossing could be more beneficial.

It would be better to cross the rail tracks with a bridge and a pathway on the south side.

No Level crossings. They are dangerous and slow down the flow of traffic.

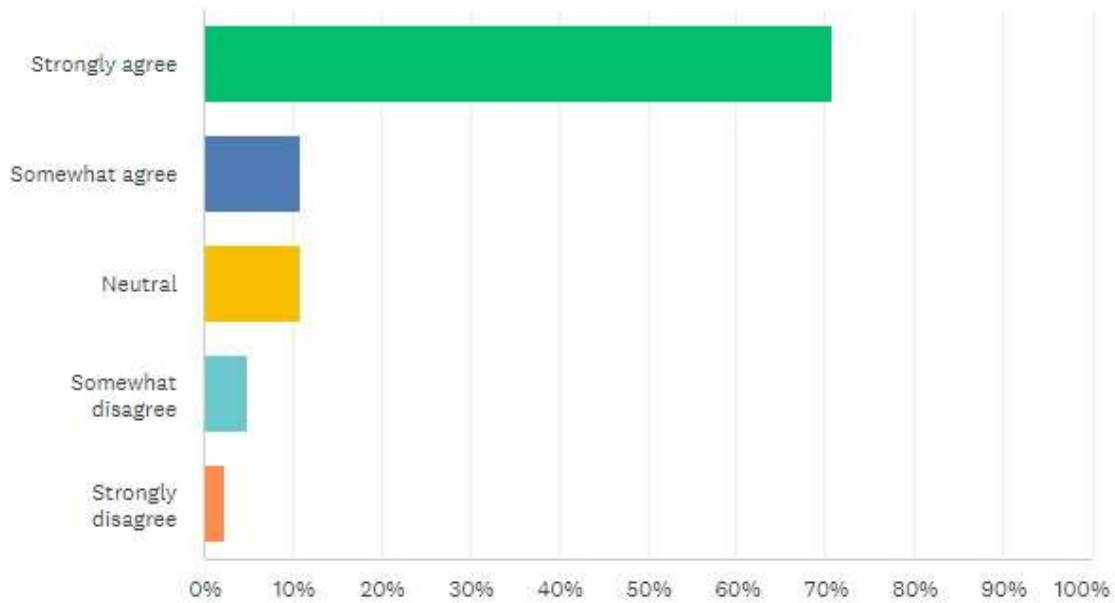
Overpass

Quoting from your own presentation from Open House #3 recap: "Section 1 recommended long-term solution: Alternative 2 – Shift Teston Road north, exiting Keele Street, GO rail overpass". Now in Open House #4 presentation it became "At-grade GO rail crossing". I'm not against the change, but please improve the presentation for average people to understand that: 1. there is a

<p>change from the previously preferred design to the current preliminary design 2. why this change took place 3. how does this impact ability of transportation for this section of the road, especially during rush hours 4. what's the plan for the overpass From the presentation, it is also not clear to me if overpass will still be considered or not in the future. If "overpass" equates to "grade separation" from the presentation, then please consider to make the slides more self-contained and more clear to normal folks who won't be familiar with these civil engineering terms without Googling or something.</p>
<p>Overpass should be constructed immediately as it will be required to support area growth. Delaying the overpass will cause inevitable issues and significant disruption to construct the overpass later. NIMBYism should not define strategy.</p>
<p>The sooner the better!</p>
<p>separation of GO Rail tracks and Teston Road as crossing is too close to Keele street and can cause traffic to back into Keele Street;</p>
<p>Before completion of the Teston Road improvements, provide additional information on future bike lane links to the North Maple Regional Park as well as future public transit routes</p>
<p>Build an overpass or underpass for GO line.</p>
<p>Stop designing the city around the car. change zoning so we can have small grocers, restaurants, hardware stores etc mixed in with residential so people do have to drive so often.</p>
<p>By shifting Teston North there is a better chance that connection to the NMRP could be completed sooner. The park use will increase significantly in the future and access will be essential. Getting this project done will significantly improve access to both the 400 hwy and the Maple Go station while minimizing travel North and South on Dufferin. I feel that whatever the quickest way to get this completed is the best option.</p>
<p>Have a centre turning lane from Keele to Rodinea, to have access to properties without disrupting traffic movement</p>
<p>Provide bussing on Dufferin St north of Major Mackenzie and to Kirby Side Road.</p>
<p>The final plan is good.</p>
<p>Get started on this project NOW!</p>
<p>The at-grade crossing at the GO rail tracks would be a lost opportunity to improve the safety of pedestrian and vehicular traffic if Teston is extended to connect Keele and Dufferin Streets. With the increased frequency of north / southbound trains for the Barrie Line and increased east / west vehicular traffic as a result of Teston Road extension, the at-grade GO crossing will cause traffic impacts to the Keele St and Teston Road intersection. The northbound Keele St right turn lane will be at capacity during times when the Barrie train is crossing at-grade. Similar conditions will occur when the east bound traffic is given priority (e.g. Green signal), and a Barrie north / southbound train is crossing at grade. The east bound traffic will end up spilling onto Keele St / Teston Road intersection as it waits for the trains to clear at-grade crossing. There should be a strong consideration to have grade separation incorporated into this design. Not doing so will create additional safety concerns for vehicular and pedestrian traffic. Further, if there is a future plan to create grade separation, we are just kicking the can further down the road and the residents at the southwest corner of Keele and Teston will need to continue to deal with the consequences of the GO Train interface. The grade separation work should be planned to coincide with the construction of the Teston to maximize the mobilization of construction crew from a scheduling and cost perspective. The slide notes that grade-separation will require easements / property for grading. The Teston Road extension has been going on for the better part of 15 to 20 years. The citizens of Vaughan would be better served to get the grade separation now...we've waited this long...might as well get it right the first time. Negotiate with Metrolinx to get the easements and get the grade separation completed together with the construction of the new road.</p>
<p>We in Vaughan need traffic calming implementation on Kirby road as the speeding , especially late night is out of control.</p>
<p></p>

Do you agree/disagree with the preliminary design recommendations for Section 2 (for example, new four-lane road, multi-use pathway on the north side of Teston Road only)?

Answered: 82 Skipped: 1



ANSWER CHOICES	RESPONSES	
Strongly agree	70.73%	58
Somewhat agree	10.98%	9
Neutral	10.98%	9
Somewhat disagree	4.88%	4
Strongly disagree	2.44%	2
TOTAL		82

Q.6 Please provide any suggestions for improvements to the Section 2 preliminary design in the text box below:

I would actually prefer to see greater interest in expanding North Maple and keeping it more natural and peaceful, rather than developing the area.

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

no

North side of Teston road has nothing wrong, but more about west side of teston road

Building a road on landfill is a complex project due to storm water and leachate. The city needs to share the detailed drainage plan and its project costs. Without a detailed cost study, people can not do a cost / benefit analysis of this road

There has been a lot of well skilled resources which went into this study regarding Section 2's preliminary design. With the rules and regulations that conforms to the Municipality of York, there should be no need for further improvements to this design for Section 2.

please provide timelines for the execution of these much needed improvements , our family strongly support these improvements

Have a pathway on the south side as well.

Would you expect any heavy traffic to any one of the nearby landfills on regular basis? If so would you consider additional right turn breakout lanes (not sure what the proper term is but hopefully you get the idea) at entrance to the landfills to prevent blocking of the 4 lanes especially during rush hours?

NO

restrict heavy truck traffic from going through residential areas

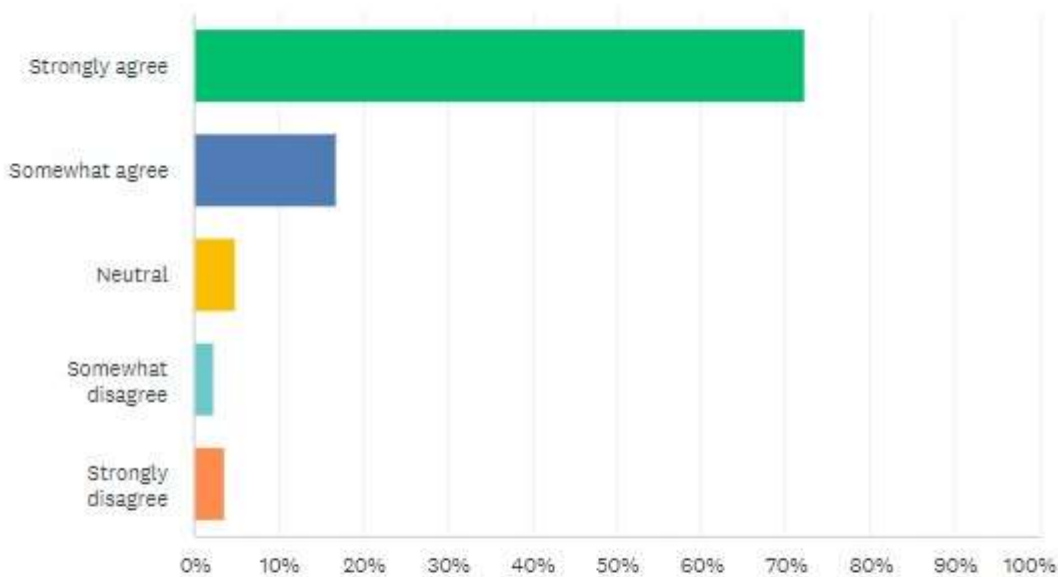
This road will be used by many people commuting by public transportation and car so sidewalks and cycle tracks on both sides would be beneficial.

Get started on this project NOW!

The bubbles indicate "future cycle track" along the southern portion of Teston Road; however in the preliminary cross sections, it only shows a track on the northern portion. The southern portion shows a 3:1 grade with a note about a wall. The bubbles are misleading and should be clarified if there is only going to be a cycling track on the northern portion and not the south.

Do you agree/disagree with the preliminary design recommendations for Section 3 (for example, new four-lane road, multi-use pathway on the north side of Teston Road only, 40-metre bridge crossing, replanting embankments, wildlife fencing and crossing, habitat replacement)?

Answered: 83 Skipped: 0



ANSWER CHOICES	RESPONSES	
Strongly agree	72.29%	60
Somewhat agree	16.87%	14
Neutral	4.82%	4
Somewhat disagree	2.41%	2
Strongly disagree	3.61%	3
TOTAL		83

Q.8 Do you have any suggestions for improvements to the Section 3 preliminary design? Please enter them using the text box below:

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

Suggest making the bridge wider to accommodate a future multi-use trail along the valley.

no

Consider including trail access from Teston shared pathway to trails beside the river on both sides of the river. There already exist trail networks both south and north of Teston in the valley. Also consider shared pathway on south side of Teston between Dufferin and the east don since this is a fairly steep section down to the river.

Building a road over a natural valley area has a significant impact on wildlife. The city should share all these environmental studies for the public

There has been a lot of well skilled resources which went into this study regarding Section 3's preliminary design. With the rules and regulations that conforms to the Municipality of York, there should be no need for further improvements to this design for Section 3.

please provide timelines for the execution of these much needed improvements , our family strongly support these improvements

Cost too much for a useless bridge, fencing, etc.

Make it architecturally appealing and less concrete on concrete.

Merging lanes to cross the bridge will cause major traffic backups just like Dufferin Street North as it arrives at teston is always backed up sometimes all the way to major Mackenzie.

NO

protecting wildlife is very important as well as using native species of plants

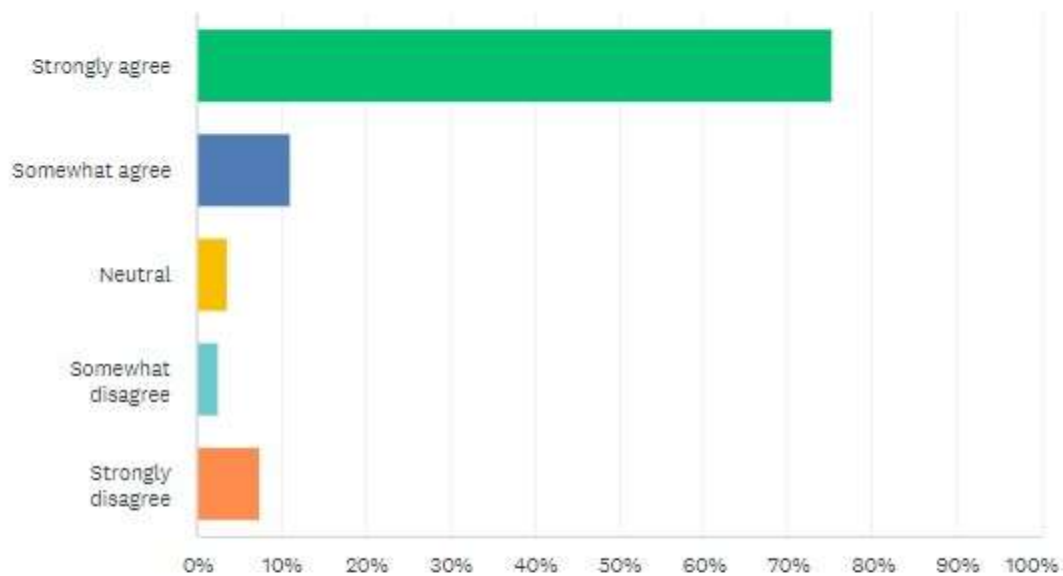
More trails into the forest would be a good.

Get started on this project NOW!

The traffic is getting bad , so Yes I agree.

Do you agree/disagree with the preliminary design recommendations for Section 4 (i.e., road widened to four lanes, sidewalks and cycle tracks on north and south)?

Answered: 81 Skipped: 2



ANSWER CHOICES	RESPONSES	
Strongly agree	75.31%	61
Somewhat agree	11.11%	9
Neutral	3.70%	3
Somewhat disagree	2.47%	2
Strongly disagree	7.41%	6
TOTAL		81

Q.10 Please provide any suggestions for improvements to the Section 4 preliminary design using the text box below:

The 800 Teston Road property, on the corner of Bathurst and Teston Road, currently zoned as rural residential, has been dangerous to enter for many years, with the roadway at it's current size. The roadway expansion will have severely adverse impacts on the ability to use and enjoy the property in a safe manner.

We own the property at 800 Teston Road, and have for over 20 years. Changes at the intersection of Teston Road and Bathurst are profoundly concerning to us. Please see followup correspondence from our son and legal represenatative, Alexander Wilkes.

While the traffic is backed up at the lights at Dufferin and Teston, beyond that, that stretch of Teston isn't very busy. I'd prefer to see improvements to the intersection at Dufferin and Teston.

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

cycle path should be separated from the main road, it would be a waste of \$ if it's just a painted line.

no

four lanes is not as that necessary, but side walk and cycle tracks are more needed

new road would be significant noise source. The city should consult with the senior home residents(Richview Manor) on noise.

There has been a lot of well skilled resources which went into this study regarding Section 4's preliminary design. With the rules and regulations that conforms to the Municipality of York, there should be no need for further improvements to this design for Section 4.

Would be great for YRT bus routes to go Westbound, through Elgin Mills to Teston Road all the way to (and past) Keele Street once the project is completed.

please provide timelines for the execution of these much needed improvements , our family strongly support these improvements

Better to extend to 4 lanes from Bathurst St to Younge St.

How will the junction of Teston/Bathurst work with 4 lanes on west side, but only 2 lanes on the east side?

1) improved noise barriers to be installed along Teston Road between Dufferin & Bathurst to reduce noise into the existing residential area. 2) 2 lanes required for left turn from Eastbound Teston to Northbound Dufferin

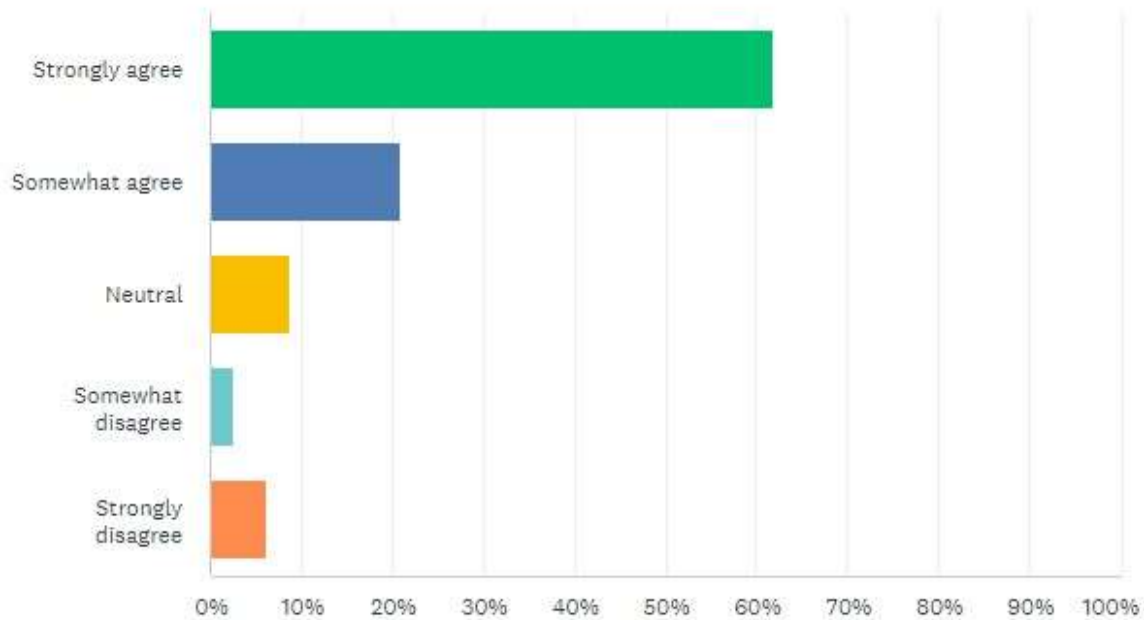
Keep a two lane road with a center turn land and add pathway for pedestrians and a separated bike lane.

need more and better transit. bus should come every ten minutes

Get started on this project NOW!

Do you agree/disagree that the proposed environmental measures appropriately address the potential environmental effects associated with the project?

Answered: 81 Skipped: 2



ANSWER CHOICES	RESPONSES	
Strongly agree	61.73%	50
Somewhat agree	20.99%	17
Neutral	8.64%	7
Somewhat disagree	2.47%	2
Strongly disagree	6.17%	5
TOTAL		81

Q.12 Please add any suggestions for improvements to the project's proposed environmental measures in the text box below:

The proposed environmental measures sound like the bare minimum. Given the prevalence and risk posed by climate change AND the general benefits of environmental protection and management, a project like this (and really any project) should demonstrate how it's going above the bare minimum. I'd like to know how it will enhance the environment and not just monitor it.

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

This study has been ongoing since 2006, think of all the time and money wasted and still have no connection to hwy400, and uncontrollable congestion.

no

Need to see significant progress, have been talked for years, if you could finalize the plan and design, the road could be built soon.

Also consider some species that might accommodate climate change better than just planting native species. Storm water ponds need to be designed better so they can serve for recreation as well and not only be cesspools for breeding Canada geese and mosquitoes.

The city needs to share detailed drainage, environment and noise study on the road and provide a project cost study before this project can move forward. The city also needs to consult with the senior home (Richview Manor) residents on noise.

The proposal is sound, and there are no additional suggestions!

I have followed and contributed to the previous open houses. It is apparent that careful consideration has been given to the environment in the project area.

Please let professionals do their work, the common residents won't have enough knowledge to make any value suggestions!!! And please start building it ASAP without further open houses!!!

huge impact on the existing green space and wild life

The presentation does not address impacts of the potential construction of high-rise residential towers at the north east corner of Teston road and Dufferin road (as stated in the York Region Plan)

Do not build wider roads, build pedestrian path and separated bike path instead.

I think it's extremely important to manage the proper maintenance of the landfill under the road, trail access connecting to the NMRP would be a huge benefit also to the local residents.

Get started on this project NOW!

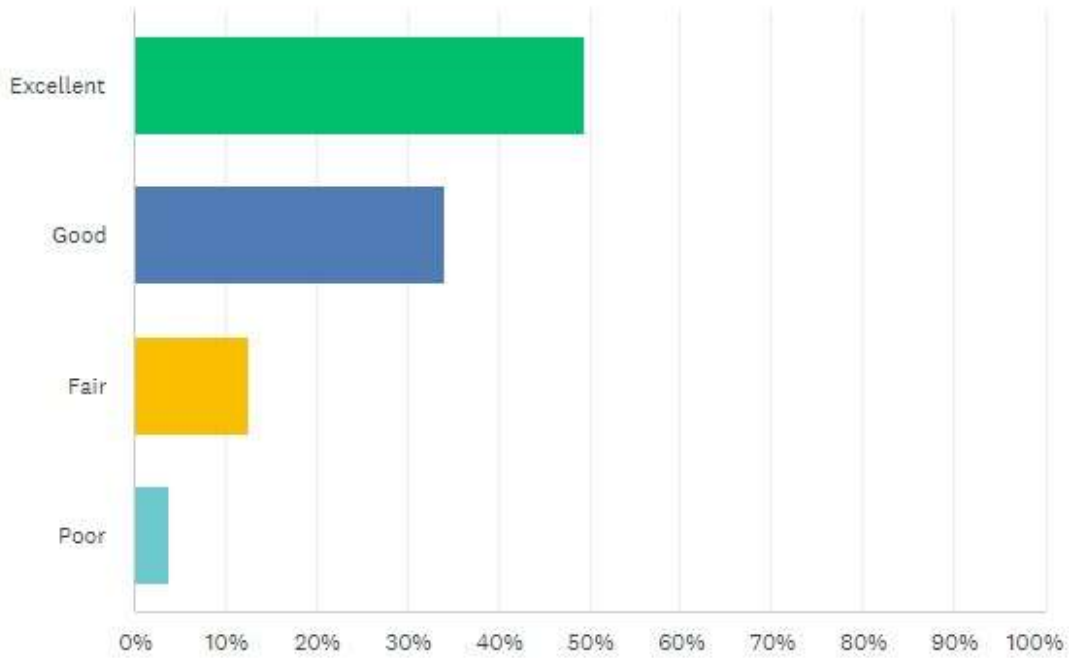
No environmental concerns.

Q.13 If you have additional comments or concerns, please enter them in the text box below:
Please see comment at item 10.
Public transportation was described in section 4 but not described for other sections. Are there any consideration to provide public transportation connections from Teston/Keele intersections through sections 1-3?
Please just extend the Teston road to the Bathurst.
Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.
Please build this ASAP. I frequent this area and the traffic is horrible.
fire all those that continued to suggest studies after studies with no useful results
no
You are studying this area since 2003. What a joke. This road should have been built a long ago. This shows how lazily this system works.
Please ensure that adequate infrastructure is in place for increased traffic. Transit is not enough in this area as Woodbridge/Vaughan has an exceptionally high per capita rate of cars. Everyone and their kid has a car!
Please reconsider Section 1 grade separation as noted in my earlier comment
I hope that the project will start quickly, which make everyone's commute more efficient and economical.
Yes, upon MECP approval PLEASE proceed with project approval on the Regional Capital Projects list, AS SOON AS POSSIBLE. Years upon years have been spent on a project that should have been an "open and shut" case. We've beaten this horse to death: once Ministry approval is received, let's EXPEDITE regional approvals and get this project done! In 2022 and 2023 Vaughan and Richmond Hill commuters have suffered from longer-than-necessary commutes due to construction on east-west arteries like Rutherford Road and Major Mackenzie Drive, which would have been much easier had Teston Road connected between Keele St. and Dufferin Rd. already.
please provide timelines for the execution of these much needed improvements , our family strongly support these improvements
At the area of future residential area (part 3, near Valley crossing) there was nesting of the Red Headed Vulture. When demolition and new development started, they were dismissed. I wonder if you are planning to build high structures for them to build new nests?
As an avid cyclist, I prefer a dedicated bike lanes at the road level that are separated from the main traffic by road marking/paint (like on Bloomington Road between Dufferin and Leslie, for example) rather than raised physically separated bike lanes (like Yonge Street from Gamble to Elgin Mills, or, further down south in Vaughan, Centre Street between Bathurst and Dufferin). The later is considerably more dangerous from my personal experience especially when crossing driveways when the risk of not being considered by the vehicles making a right turn is extremely high (am having regular occurrences of this myself at least few times a year).
Can't wait for this road improvement, it is well needed!
Anxious to see this come to fruition. This will limit congestion and reduce timelines to get across Teston!
This project should be prioritized by the ministry and Vaughan since access to the Go station would be easier than traveling North and South on Dufferin and Keele. This is a much better use of tax dollars than connecting Kirby from Dufferin to Bathurst. Vaughn should work with York Region on the projects. This would be a better outcome for the residents and would minimize effects to the environment and use of tax dollars. Increased public transportation should also be improved along with this process.
Whichever method will get this project done the quickest I would strongly agree with it. We badly need this connection in the neighbourhood to ease traffic congestion.
This project is long overdue. Please build it asap!!!

Reinforcing the fact that the proposed at-grade GO transit crossing is short sighted. It will cause further safety issues at the intersection of Keele and Teston, especially when the east / west connection of Teston is opened. Since we're planning to construct a new road, there should be consideration to do the grade separation at the same time to minimize disruptions in the future and maximize the efficiencies of construction occurring already.

To what degree did this virtual open house meet your information needs?

Answered: 79 Skipped: 4



ANSWER CHOICES	RESPONSES	
Excellent	49.37%	39
Good	34.18%	27
Fair	12.66%	10
Poor	3.80%	3
TOTAL		79

Q.15 Please provide any suggestions for improvements to future virtual open houses in the text box below:

We need far more detail on any proposed changes at our property.

The presentation was very technical and did not provide the information in an accessible way to a layperson. The feedback could be improved with something more interactive that took participants through the design proposals with better visuals and explanations, and also provides them with the opportunity to answer questions related to that topic. It was a lot of information to retain and then also answer questions at a later time.

Only one suggestion: Please make Teston road connection as fast as possible and as wide as possible all the way from Keele Street to Yonge Street. Debates are going for over two decades.

When all the changes are finalized, recommend providing a one or two page snapshot of current and future state. Don't forget to include survey responses of percentage approval Thanks

no

Get the word out. I only found about this project by accident on a Next Door post.

I suggest have another open house to address the concern I have.

Let's get this project started, completed, and move on, please!

Less jargon and more plain language.

With a virtual open house allows me to be safe at home or at the office without getting exposed to the Corona Virus, and if I don't have transportation to go to the open house location. It makes things more convenient to collaborate via a virtual open house remotely, and not to mention saves on gas.

Technical terms could be linked in an interactive format (PDF) with links to explanations of said terms.

This is a prime example of how civil servants waste time and money - the Egyptians built the pyramids in less time than the Govt is pissing around wasting time and money with all of the presentations and surveys.

Please make the presentation more self-contained and better explained for average folks, especially if there are changes made between Open House #3 and Open House #4.

It would be nice if there is a scheduled virtual open house meeting in which the municipal representatives would be available to discuss with the residents of the area the contents of the YouTube video and the presentation slides

Get started on this project NOW!

Provide more clarity into the IEA process. What happens after the MECP approves the IEA? What happens if the MECP does not approve the IEA? Also, provide clarity beyond the IEA and how it relates back to York Region's responsibilities after IEA approval.

Need more specifics on what is transpiring with proposed dates